

CALVERT COUNTY
DEPARTMENT OF PLANNING & ZONING

Prince Frederick, Maryland 20678
(301) 535-1600/(301) 855-1243 (DC)

Director
Frank A. Jaklitsch

January 25, 1985

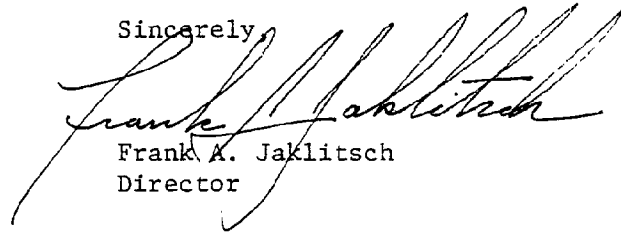
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TO: Persons Interested in the Solomons Draft Master Plan

Attached is a copy of the Solomons Draft Master Plan, prepared by the consultants. We would appreciate your submitting any comments you may have regarding the Plan to the Department of Planning & Zoning by March 25, 1985, so that they may be considered while revising the Plan.

The Planning Commission's Public Hearing has not yet been scheduled, but probably will be held late spring. Before the public hearing, the Department of Planning & Zoning is expected to hold a meeting in Solomons to explain the Plan and to answer questions.

Sincerely,

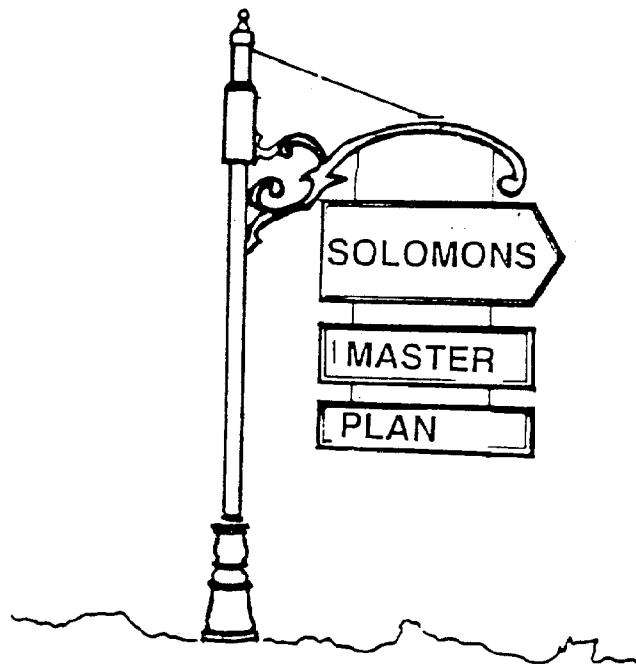


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Preface

No community can plan successfully without creating a shared vision for the future. The recommendations in this Master Plan are due to the work and dedication of many individuals and groups. In particular, we wish to acknowledge the major contributions of all those residents who participated in the series of public meetings designed to identify problems and to comment upon our proposed solutions. We particularly wish to thank those citizens and the Solomons Civic Association who wrote letters and submitted their suggestions which form such a key component of this Plan. Our special thanks are extended to those curators of Solomon's history at the Calvert Marine Museum, and especially to Dr. Ralph Eshleman who so generously gave of his pictures of the past. We also remember with pleasure an exhilarating afternoon studying Solomons by sea, courtesy of Skip Zahniser. No progress would be possible without the support and cooperation of the agency representatives. We wish particularly to thank the Frank Jaklitsch, Sonny Dowell and Greg Bowen of the Calvert County Department of Planning.

The Phipps Group has been responsible for the overall direction and management of the Master Plan for Solomons. J.M. Hutto Associates prepared the analysis and guidelines for waterfront development, while Alan Sparber & Associates, Architects have been able to translate general development goals into graphic blueprints for the future.

The Phipps Group
J.M. Hutto Associates
Alan Sparber & Associates, AIA.

CHAPTER I

INTRODUCTION

CHAPTER I
INTRODUCTION

- A. Purpose of the Solomons Master Plan**
- B. Existing Conditions**
 - 1. Geographical Location
 - 2. Demographics and Economics
- C. Background**
 - 1. Historical Overview
 - 2. Calvert County Comprehensive Plan:
Town Center Zone
 - 3. Community Concensus Process
- D. Planning Policies**
 - 1. Relevant Comprehensive Plan Objectives
& Recommendations
 - 2. Master Plan Principles
 - 3. Timing of Review of this Master Plan

A. PURPOSE OF THE SOLOMONS MASTER PLAN.

The purpose of this Master Plan is to provide comprehensive yet flexible guidelines which will help direct future growth and development within the Solomon's Town Center. It is designed to address the issues of protection and enhancement of the residential areas of Solomons, as well as the encouragement



fg.1

SOLOMONS MASTER PLAN

PREPARED FOR CALVERT COUNTY BY THE PHIPPS GROUP, J.M. HUTTO ASSOCIATES AND ALAN SPARBER & ASSOCIATES

Scenes in
Solomons

of compatible commercial and marine development along the Patuxent River, Back Creek, and Routes 2-4.

Preservation of Solomon's water resources heritage and design character is an important component of this Master Plan. Thus, it includes not only general guidance for dealing with overall growth management, but specific appearance guidelines, a general land use plan, and an implementation schedule. Furthermore, it graphically illustrates where the public interest encourages significant public expenditures, and where the public interest would like to see private development occur.

B. EXISTING CONDITIONS.

1. Geographical Location.

Solomon's Island is located at the southern tip of Calvert County at the mouth of the Patuxent River. This portion of Calvert County is a peninsula bordered by the Patuxent River on the west and the Chesapeake Bay on the east. The Island itself, in conjunction with the Dowell Peninsula and Lusby Point, forms a deep protected harbor which has served shipping since the early Eighteenth Century.

The Solomon's Town Center designation incorporates the land to the north of the Island along Maryland Routes 2/4, and the majority of the Dowell Peninsula. (See Town Center Boundary Map, this Chapter).

Access to the area is provided from the north by Highways 2/4, and by two bridges spanning the Patuxent Rivers - one crossing from Charles County via Route 231 approximately 20 miles to the north, and the other connecting Solomons with St. Mary's County to the west via Route 2/4 over the Johnson Memorial Bridge.

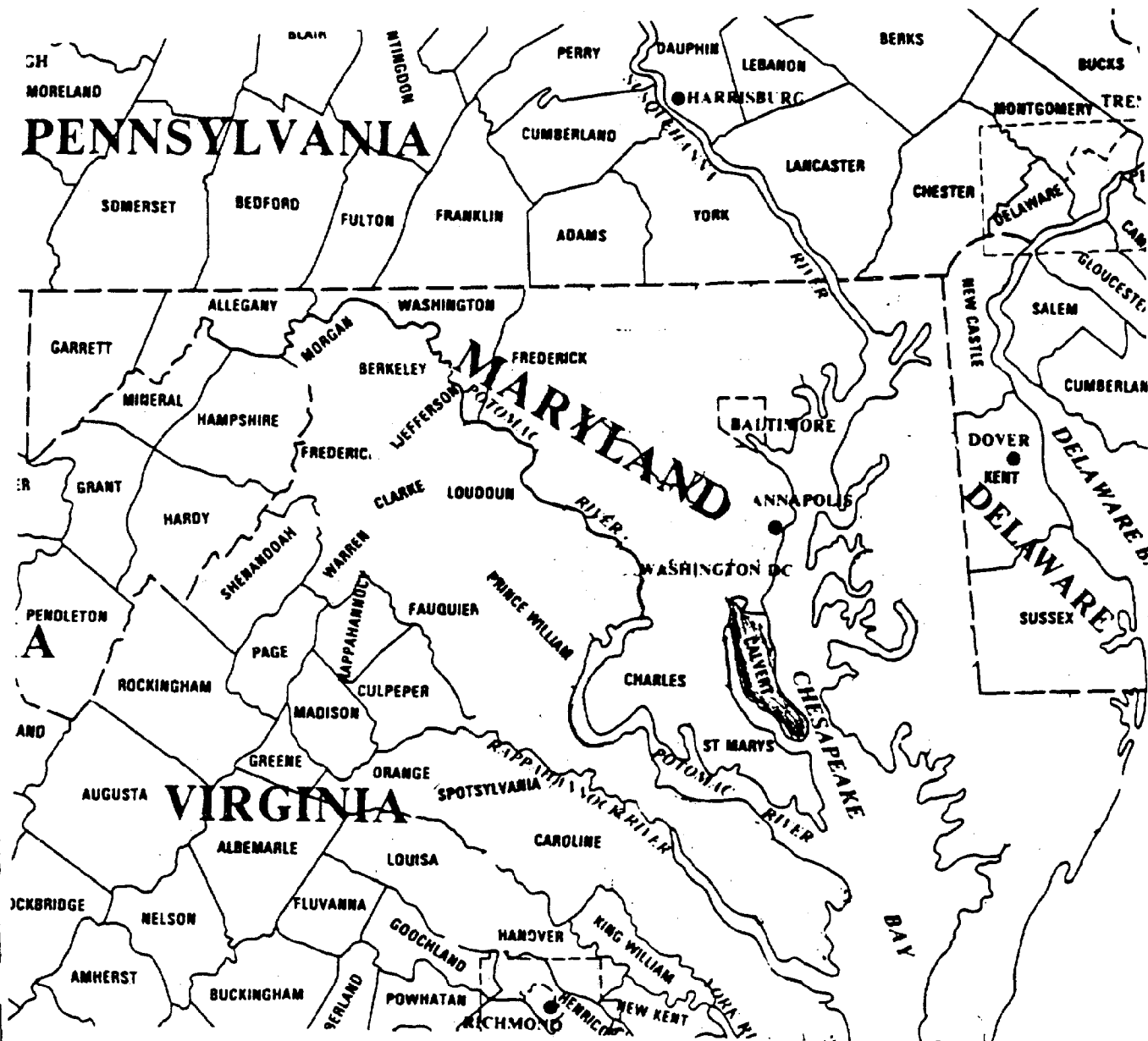


fig. 2

SOLOMONS MASTER PLAN

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LOCATION

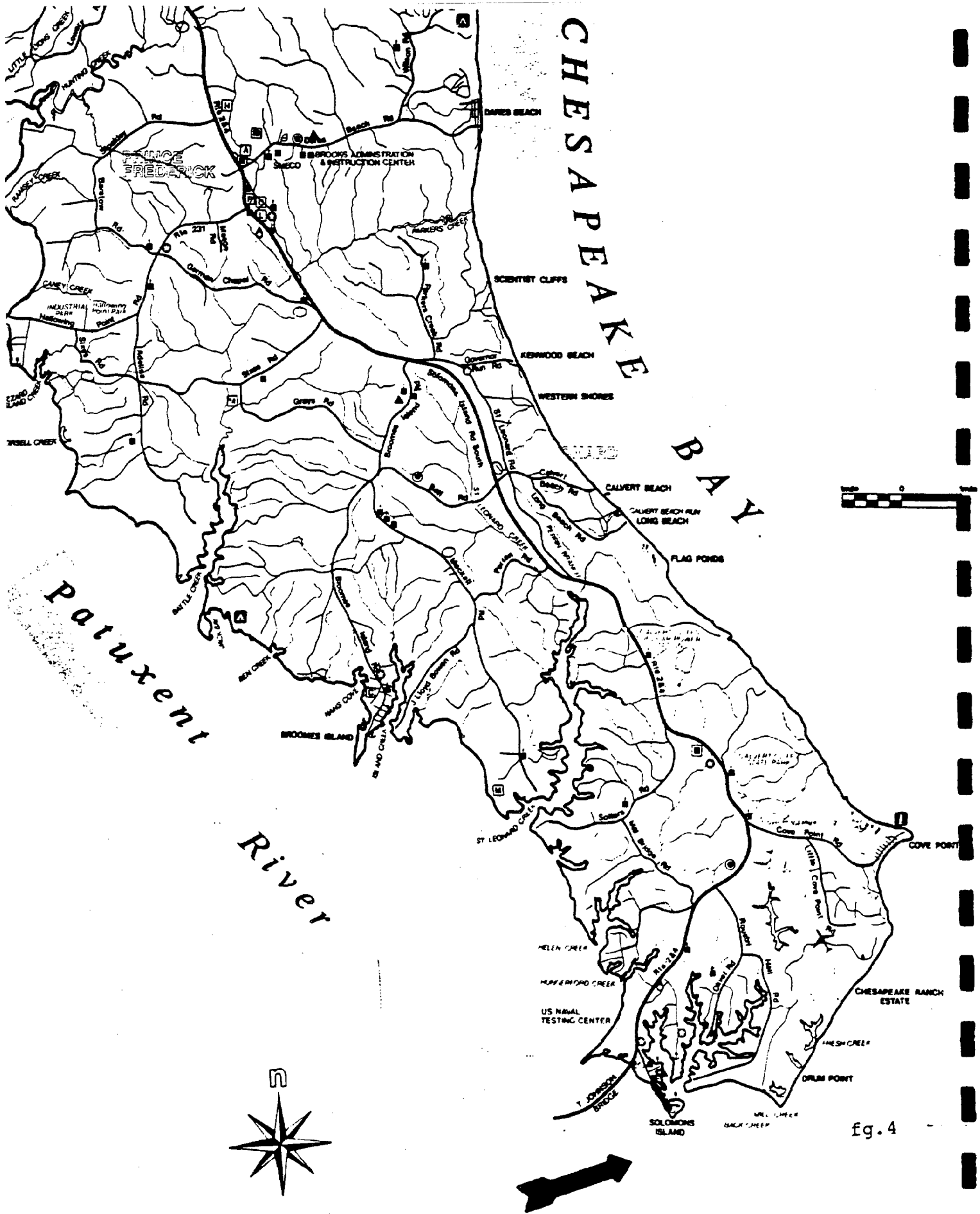


fig. 4

SOLOMONS MASTER PLAN

Location in
Calvert County

2. Demographics and Economics.

In 1980, Solomon's had a population of 700, which represents 2% of the Calvert County population of 34,636.¹

Population projections are for the County to increase by 4% annually to a year 2000 total of 62,358.²

The State of Maryland grew at an average annual rate of 0.7% from 1970 to 1980. Calvert County accounted for 4.8% of that growth, increasing from a population of 20,682 in 1970, to 34,636 in 1980, for a 6.7% annual rate of increase.

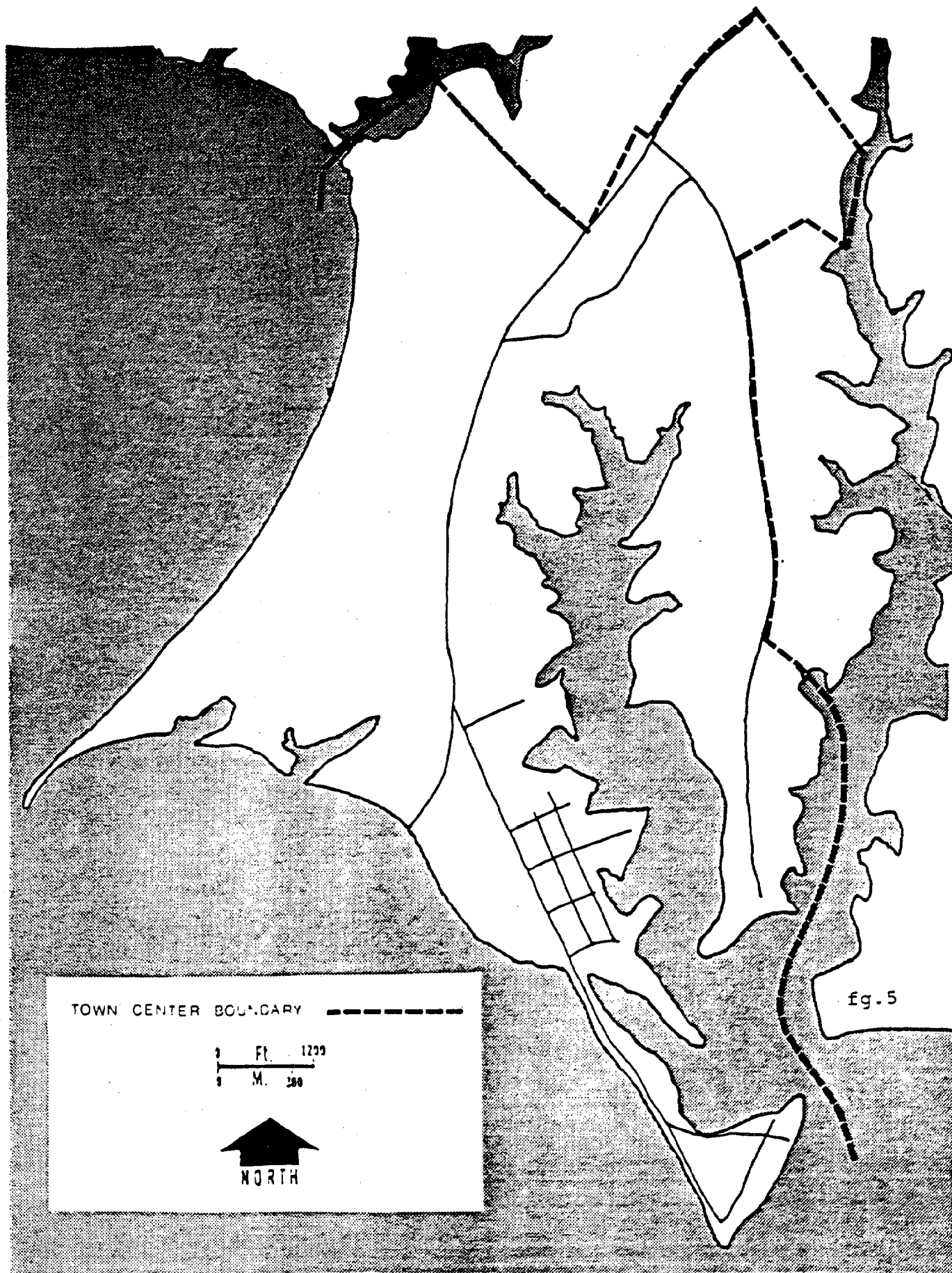
Population projections indicate that Maryland will continue to grow at 0.8% annually through the year 2000. Calvert County is projected to capture 4.3% of that growth.

An analysis of the age distribution in Solomons reveals that while 55% of the population is under the age of 44 years, a considerably higher ratio of people over 45 (40%) live in Solomons than in Calvert County (26%), Maryland (30%), or the Washington D.C. SMSA (27%).

In 1979, the median household income in Calvert County was \$22,106 with 41% of the 10,732 households earning \$25,000 or more. Solomons, on the other hand, had a 1979 median household income of \$16,453, 24% below the median for the County. This is caused, in part, by the cyclicity of the tourist business in Solomons. Approximately 14% earned between \$20,000 - \$25,000, and 29% between \$25,000 - \$50,000.

¹U.S. Department of Commerce, Bureau of the Census.

²State of Maryland, Calvert County, population projection by County.



TOWN CENTER BOUNDARY

0 FT. 1200
0 M. 200



fig. 5

SOLOMONS MASTER PLAN.

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TOWN CENTER
BOUNDARY

C. BACKGROUND.

1. Historical Overview.

Originally called Somervell's Island (1740-1814), Sandy Island (1827-1865), and Bourne's Island (c.1860), this area finally became known as Solomons Island because of Issac Solomon's oyster packing facilities following the Civil War.³

The oystering industry was supported by the development of shipyards on the Island in the late 19th century. By 1880, the fishing fleet at Solomons exceeded 500 boats, and the famed "Bugeye" sailing craft was developed to support the Island's fishing fleet.

Because of the Solomons deep harbor and uncomplicated entry from the Patuxent, ships have always played a major role in the Island's history. On the Patuxent side of the Island is one of the great natural harbors of the East Coast - over two miles wide and in places over one hundred feet deep. In the War of 1812, Commodore Joshua Barney's flotilla sailed from the Solomons to attack the British fleet in the Bay. And over 200 years later, the U.S. Navy established a World War II Naval Amphibious Training Base on the stretch of land lying between Solomons' Mill and Back Creeks to prepare American troops for overseas assaults.

Across the River is the Patuxent Naval Air Test Station which from 1941 until 1977 was connected to Solomons by ferry service which saved its passengers a 60 mile one-way commute up Calvert County, across the Patuxent River Bridge at Benedict, and back down the St. Mary's County side. The opening of the Johnson Memorial Bridge in 1977 caused the end of the ferry service.

Following the War, extensive marina development

³R. Eshelman and C.M. Dixon, Historical Tours through Southern Maryland, 1983, p.3.

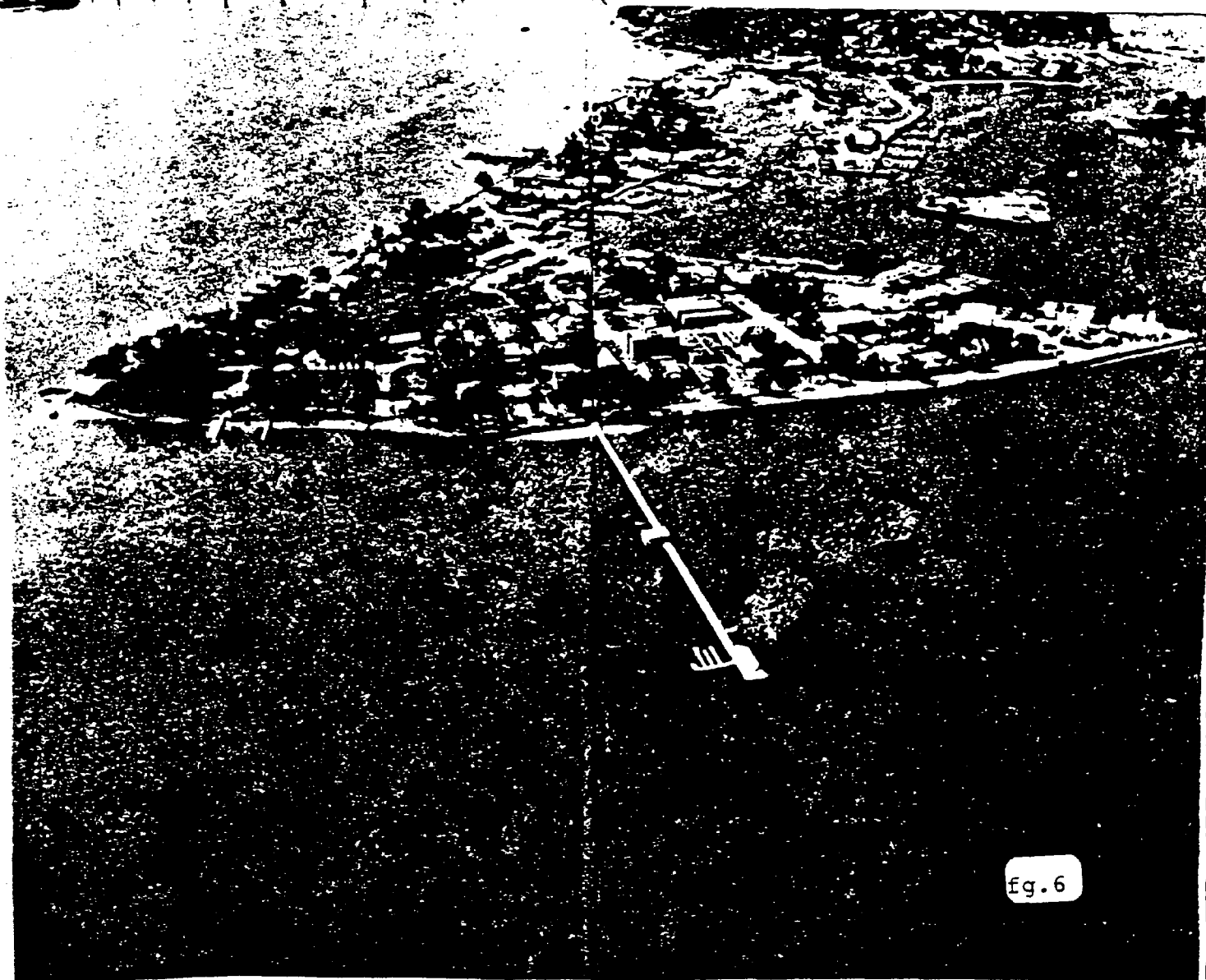


fig. 6

SOLOMONS MASTER PLAN

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SOLOMONS

was begun in the area, until today over 11 separate marinas offer a total of 1,241 slips for transient and long-term dockage.

2. Calvert County Comprehensive Plan : Town Center Zone.

The purpose of the Calvert County Comprehensive Plan is "to ensure that Calvert County maintains the capability to guide development so that the nature and character of the County may be preserved for future generations."⁴ The Comprehensive Plan was approved February 10, 1983 and adopted on June 28, of the same year.

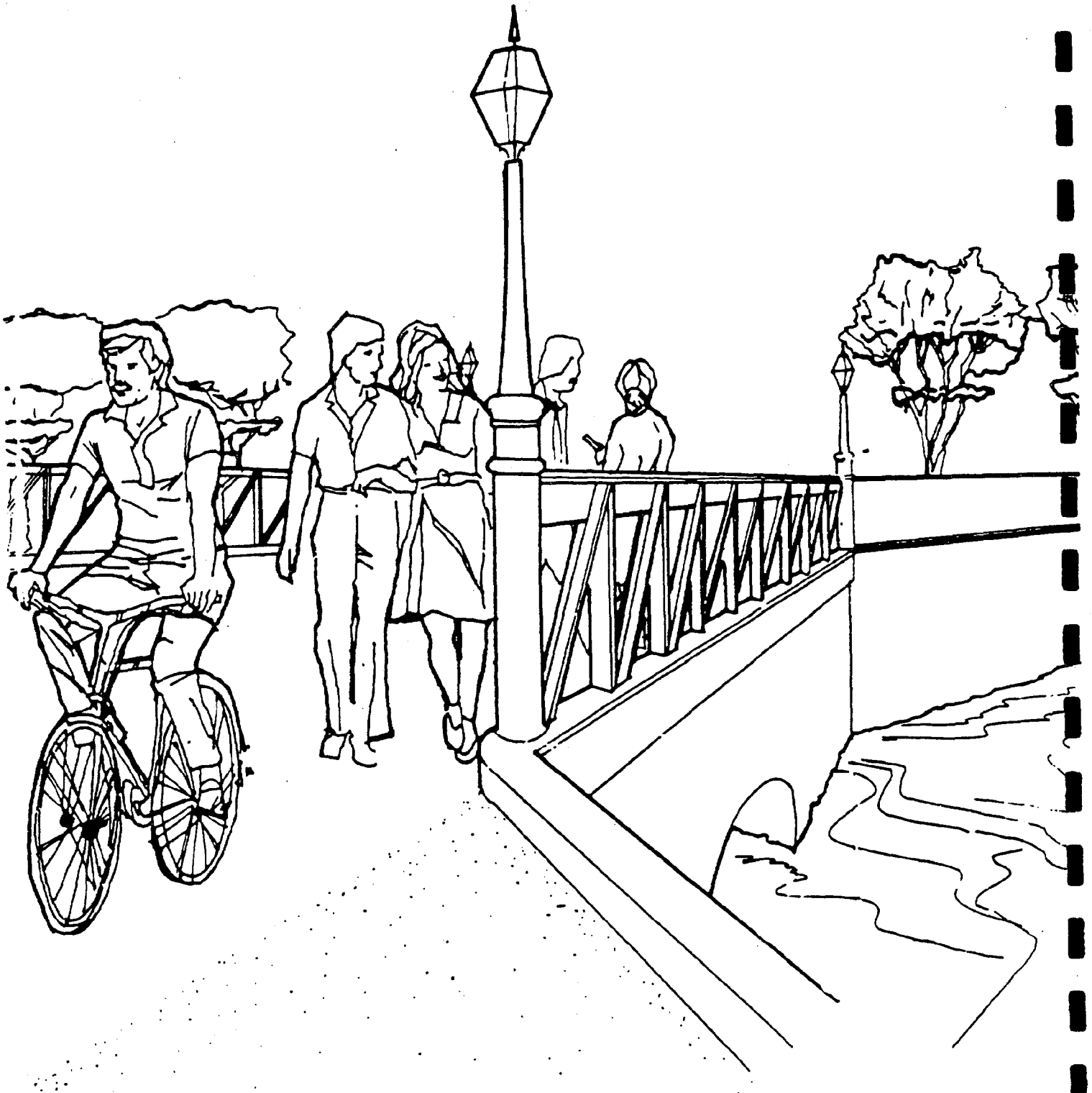
Central to the concept of guiding development for the future was the development of the Town Center Zone, a land use category whose purposes are to:

1. Concentrate commercial growth and employment opportunities in Town Centers,
2. Encourage multi-family residential development to locate in Major Towns,
3. Encourage single-family residential development to locate in Major Towns,
4. Promote efficient and attractive development in a rural setting,
5. Allow increased residential densities in Towns by locating Agricultural and Land Preservation Transfer Zones in these areas.⁵

Proposed development within a Town Center must be consistent with a Master Plan whose function is to clearly delineate types of appropriate land uses and the suitable scale of development.

⁴Calvert County Comprehensive Plan, Approved and adopted, 1983, p.1.

⁵Comprehensive Plan, p. 13.



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SOLOMONS MASTER PLAN

PREPARED FOR CALVERT COUNTY BY THE PHIPPS GROUP, J.M. HUTTO ASSOCIATES AND ALAN SPARBER & ASSOCIATES

ENTRY BRIDGE

The process for the development and approval of a Master Plan must:

1. Implement an active Citizen Participation Program,
2. Establish land use policies based on desirable economic development, public facilities available and needed, residential development, aesthetics, compatibility with and protection of existing and previously planned uses, and the cumulative effect upon the County.
3. Delineate areas to have certain public facilities, including community water and sewerage and roads,
4. Designate the physical boundaries of the Towns based on natural features, existing land uses, and policies indicated above.
5. Delineate identified locations not suited for certain types of development and restrict such development.

3. Community Consensus Process

As preparation for the development of this Master Plan, two community forums were held by the Calvert County Planning Staff to determine a general consensus on needs and goals for the area.

At the first meeting, held on April 28, 1983, much of the discussion focused on the critical question of compatibility between residential and commercial uses in the area of Lore Street south, with significant concern voiced over control of public drinking and noise, overnight camping and fishing along the bulkhead, and the proliferation of liquor licenses.

A second major category of concern, voiced at this meeting, dealt with the issues of growth management and preservation of the unique character of the community. This was expressed in the discussions dealing with new commercial development along the bulkhead and the need for parking, the question of height limitations, and the desire for architectural appearance guidelines.

On July 30, 1984, a second meeting was held with the community. There was a renewed insistence on control of parking, trash, and speeding problems along the bulkhead; a renewed request for architectural standards; a concern about potential traffic and access problems on Dowell Road; and identification of traffic problems at the Patuxent River Bridge and Lore Street intersection. A height limitation of 3 stories was proposed for the entire Town Center area.

These community concerns form the basis for the planning approach to this Master Plan for Solomons. (Reports on these two community meetings and the issue rankings are included in Appendix A).

D. PLANNING POLICIES.

The recommendations incorporated into this Solomons Master Plan reflect and support the planning policies developed over the years in Calvert County. In particular, this plan seeks to accomplish the County's goals and objectives as documented in the Comprehensive Plan.

1. RELEVANT COMPREHENSIVE PLAN OBJECTIVES & RECOMMENDATIONS.

The Comprehensive Plan objectives relevant to the Solomons' Master Plan are the following:

a. Land Use:

- o encourage new residential development to locate in Towns; encourage institutions and businesses to locate in Town Centers,
- o provide for efficient uses of land, environmental safeguards, and high quality site designs through the use of flexible development controls; density transfer, cluster development, historic district zones, and site plan review,
- o designate adequate areas for commercial use in the Town Center.

b. Public Facilities:

- o Reinforce Town Centers by identifying appropriate public facilities - sidewalks, street lights, curbing, landscaping, public squares, etc. - and providing or requiring provision of such facilities.

c. Historic Preservation Controls:

- o Encourage multi-family, commercial and industrial site design to be visually compatible with surrounding areas that have been designated historically or culturally significant.

d. Farmland Retention:

- o encourage and support the marketing of county produced farm products by providing farmers markets within Town Centers.

h. Town Center Recreational Space:

- o Recreational open space should be provided within Town Centers. It should be attractive and useful - with places to sit and relax, and to present shows and ceremonies. It should be near or at the center of activity.
- o provide and or regulate public squares, pedestrian walkways and safe bicycle routes within Town Centers.
- o establish a centralized source of information regarding recreation-related programs.

i. Economic Development:

- o provide a legal mechanism for establishing public facilities construction districts which would permit, under specific circumstances, the development of streets, sewer, water and related public facilities in Town Centers. Under this provision, the County Commissioners should be empowered to levy a benefit charge.
- o promote the development of tourist attractions within the County and encourage the development of lodging accommodations and other tourist related facilities.
- o promote retail outlets, including public market places, in Town Centers for agricultural and seafood industries.

2. MASTER PLAN PRINCIPLES.

The underlying issue, which this Master Plan has attempted to address, is how can the natural assets of the area, i.e., the water, the location, and the flavor of Solomons be developed, protected and promoted so that the character and the nature of the area may be preserved for future generations.

To achieve this goal, six policies were followed:

- o The entire Town Center area of Solomons, while composed of six specific geographic areas, should be treated as an economic and aesthetic whole, and appearance guidelines should apply throughout.
- o All the water-based activities of Solomons, including charter boating, recreational boating, crabbing and fishing, should be recognized and provided for within the master plan.
- o A "sense of place" should be recreated in the historic commercial core of Solomons.
- o Established residential areas should be protected and enhanced.
- o Compatible commercial development should be encouraged.
- o Flexibility should be allowed in terms of land use designation on major undeveloped parcels, so that a lively mixture of uses may develop in response to market demand and appearance guidelines.

3. TIMING OF REVIEW OF THIS MASTER PLAN.

No plan is ever final or definitive. It reflects current conditions and anticipates future developments. It's function is to provide policy guidance and site specific recommendations as a planning framework within which change may occur.

The Solomon's area has been identified as a high growth area in which a significant amount of pent-up residential and commercial demand is presumed. As conditions change over time, it would be appropriate to review and update these Master Plan recommendations - in 5 to 10 years.

In addition, it is clear that the major public improvements proposed will be implemented in specific stages. Any plan which presumes simultaneous implementation of all recommendations, involving either public funding or private investment, would be unrealistic.

This plan continues a process of change in Solomons which has been occurring since its initial founding in the Seventeenth Century. Over time, the Island has been historically reshaped to reflect the changing economic conditions of the water-based industries which lay at the core of its history.

CHAPTER II

LAND USE RECOMMENDATIONS

CHAPTER II

LAND USE RECOMMENDATIONS

A. Introduction

1. Six Planning Areas
2. General Recommendations for Town Center.

B. Sandy Point Area

1. Objectives
2. Considerations
3. Recommendations

C. Charles Street to Lore Street

1. Objectives
2. Considerations
3. Recommendations

D. Lore Street to Dowell Road

1. Objectives
2. Considerations
3. Recommendations

E. Dowell Peninsula Area

1. Objectives
2. Considerations
3. Recommendations

F. Swagers Point Area

1. Objectives
2. Considerations
3. Recommendations

G. St. Johns Creek Area

1. Objectives
2. Considerations
3. Recommendations

A. INTRODUCTION.

This chapter divides the Solomons Town Center into six specific planning areas - with their own needs and constraints - and discusses objectives, special considerations, and recommendations for each.

These planning areas reflect a commonality of geography, environmental considerations, or established land use patterns, and have been defined by the expressed needs of the community and the general Master Plan policies identified in Chapter I.

While the thrust of the Master Plan is to treat Solomons Town Center as an economic and aesthetic whole, it is important that land use recommendations be tailored to the unique characteristics of the various planning areas within the Town Center.

1. SIX PLANNING AREAS:

The six planning areas are:

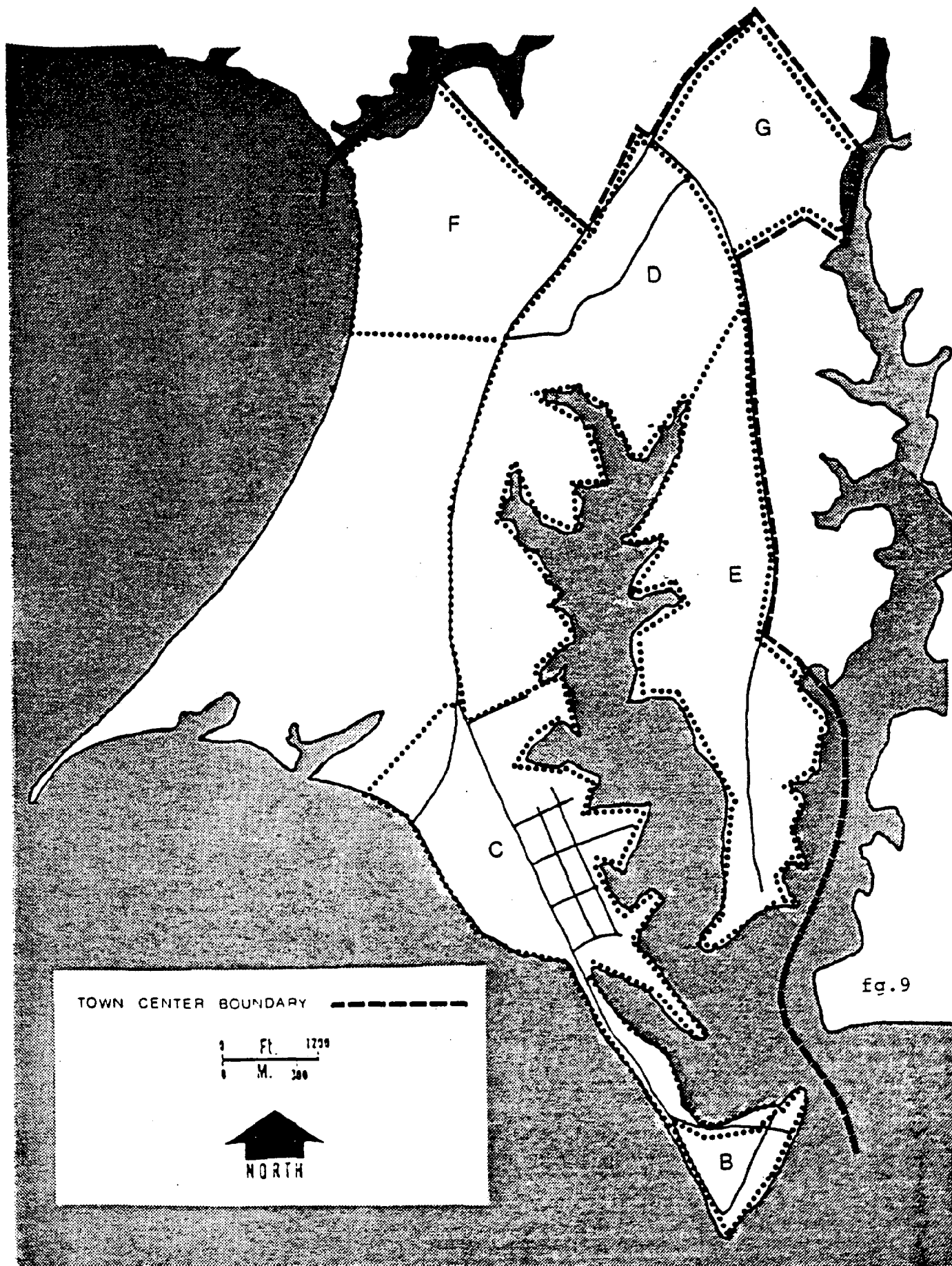
- o Sandy Point area (Planning Area B)
- o Charles Street to Lore Street area (Planning Area C)

- o Lore Street to Dowell Road (Planning Area D)
- o Dowell Peninsula area (Planning Area E)
- o Swaggers Point area (Planning Area F)
- o St. Johns Creek area (Planning Area G)

Figure 9 is a map of the Town Center showing the boundaries of these planning areas. The planning objectives, considerations and general land use recommendations for each planning area in the Solomons Town Center are presented in this chapter. Each planning area (with the exception of Planning Area G) is further subdivided into planning subareas. Precise land use boundaries for each subarea are designated on the maps for each area by alphabetical and numerical symbols. This system was designed to clearly delineate where specific permitted and conditional land uses would be allowed and encouraged. A table of these permitted and conditional land uses by geographic subarea as well as a list of special development requirements is provided in Chapter III.

2. GENERAL RECOMMENDATIONS FOR SOLOMONS TOWN CENTER:

- o This Master Plan has been designed to encourage the mixture of uses within the historic commercial core of Solomons, and to protect and enhance the existing residential neighborhoods. It allows for significant residential and commercial development to occur within the remainder of the Town Center area, in accordance with appearance and setback standards.
- o In order to retain the small town character of Solomons Island, height limits of 35 feet including the roof should be observed from Lore Street south. North of Lore Street, within the remainder of the Town



SOLOMONS MASTER PLAN

PREPARED FOR CALVERT COUNTY BY THE PHIPPS GROUP, J.M. HUTTO ASSOCIATES AND ALAN SPARBER & ASSOCIATES

PLANNING AREAS

Center, a height limit of 50 feet including the roof should be observed.

- o Appearance guidelines have been developed for the Town Center. The guidelines will apply to all new construction or rehabilitation of the exterior of structures.
- o Waterfront development guidelines have been developed and will apply to all waterfront development and rehabilitation. A construction setback of 100 feet is recommended for all new development north of Lore Street. Height and setback requirements are listed in the text.
- o A design theme for public improvements had been developed which pertains to street furniture and lighting, street signage and other public facilities.
- o A bicycle path has been designated for the entire Town Center area to provide an alternative means of transportation. The path would connect the residential, recreational and parking areas with the commercial areas.
- o All pedestrian circulation and automobile traffic should be separated and clearly defined.
- o Guidelines for business directional signs have been developed along with procedures for implementation.

B. SANDY POINT AREA. (Planning Area B, including Planning Subareas B1,B2,B3,B4)

The Sandy Point area identified in Figure 10 includes the Sandy Point residential area; the institutional buildings and property of the University of Maryland Biological Laboratory; a mixture of residential, marina and industrial buildings and structures on property located at the north end of Farren Avenue; and the retail and motel uses in the center of the area.

1. **OBJECTIVES:** The objectives to be accomplished by the land use recommendations are:

- o To preserve and enhance the residential quality of Sandy Point, while encouraging compatible commercial uses along Charles Street.
- o To encourage the continuation of the campus-like character of the Biological Laboratory property.
- o To preserve and provide accessibilty to the scenic views of the water.

2. **CONSIDERATIONS:** The special environmental and land use considerations affecting this area are:

- o Sandy Point is an established residential area.
- o Except for a small area of former marsh land in Subarea B4, the rest of the area is fully developed.

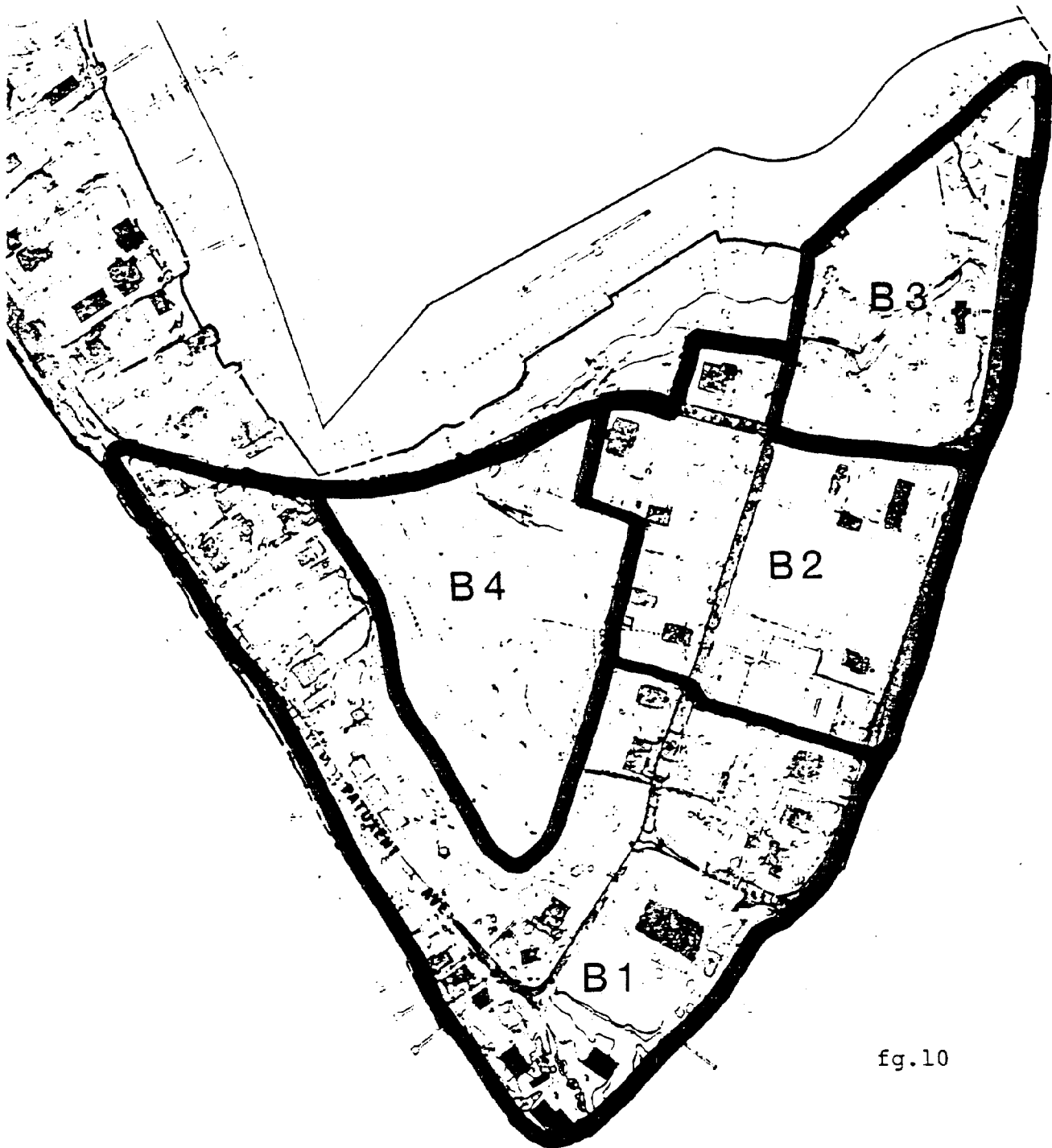


fig.10

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**Sandy Point
Planning Area B**

- o There are very attractive water views from Farren Avenue and Charles Street.
- o The land on the far northern tip of the Island, Subarea B3, is located near the mouth of Back Creek as well as the Patuxent River and has good water depths at the entrance to the harbor.
- o This area has been settled for many years and much of the shoreline has been bulkheaded or stabilized in some manner thus destroying the wetlands which would have normally occurred here.
- o Rates of shoreline erosion vary widely within this subarea between very high to very low. However, most of the higher erosion areas have been stabilized.
- o The Biological Laboratory has had a long term presence in the area.
- o There is a mix of uses along Charles Street. The western portion of the Street needs visual improvement, while the eastern portion of the street is very attractive.
- o While the main island road terminates at Farren Avenue, there is no turnaround for vehicles.
- o There have been complaints about excessive noise in the commercial area.
- o There is an existing stormwater runoff problem in Subarea B4, the center of the area.

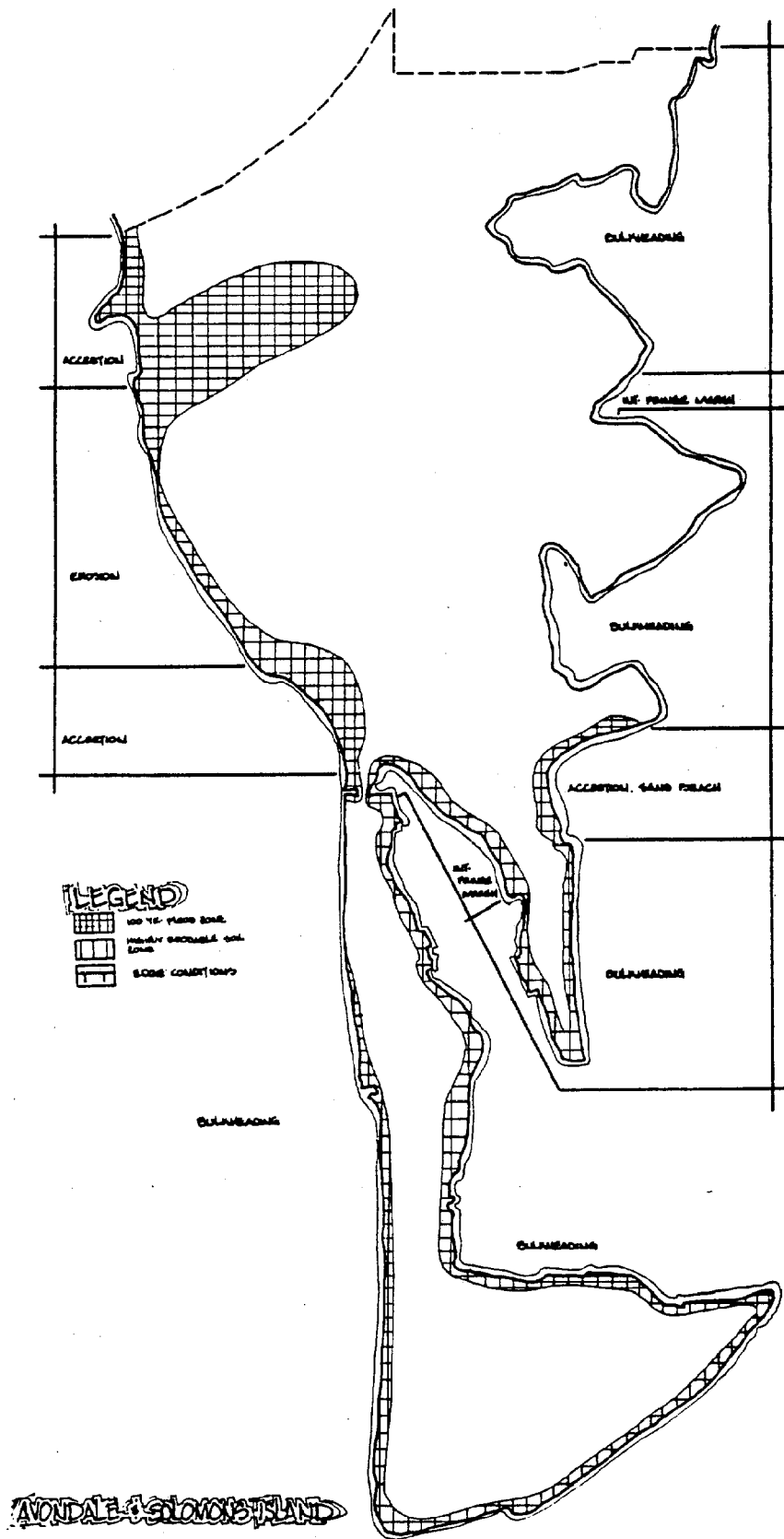


fig.11

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Shore Profile
Areas B & C

3. **RECOMMENDATIONS:**

- o The Sandy Point residential area along Patuxent Avenue, including residential property along Williams Street and Farren Avenue should be designated for residential use only. (Subarea B1).
- o To protect this residential area, the entrance to Sandy Point from Solomons Island Road at Patuxent Avenue should be modified to discourage through traffic. Details of this recommendation are given in Chapter V, Public Improvements.
- o The westerly portion of Maltby Street shown on land maps but not currently being used should be officially abandoned to protect the residential area.
- o The property owned by the Biological Laboratory, Subarea B2, should be designated for institutional use. There should be adherence to the appearance standards governing new development and substantial rehabilitation in order to preserve the architectural character of the area and to ensure compatibility with adjacent residential areas.
- o A new mixture of water-related residential and commercial uses should be encouraged in the area between Farren Avenue, Williams Street and north of Charles Street, Subarea B3. The current industrial oil storage use and vertical boat storage is not the most appropriate use for the main water entrance to Solomons, and in the long run should be phased out.
- o The attractive tree-lined character of eastern Charles Street should be preserved. This can be accomplished through retention of residential use or residentially oriented use (bed and breakfast). Limited commercial use of rehabilitated structures would be permitted under special exception and strict design review procedures

in Subarea B2.

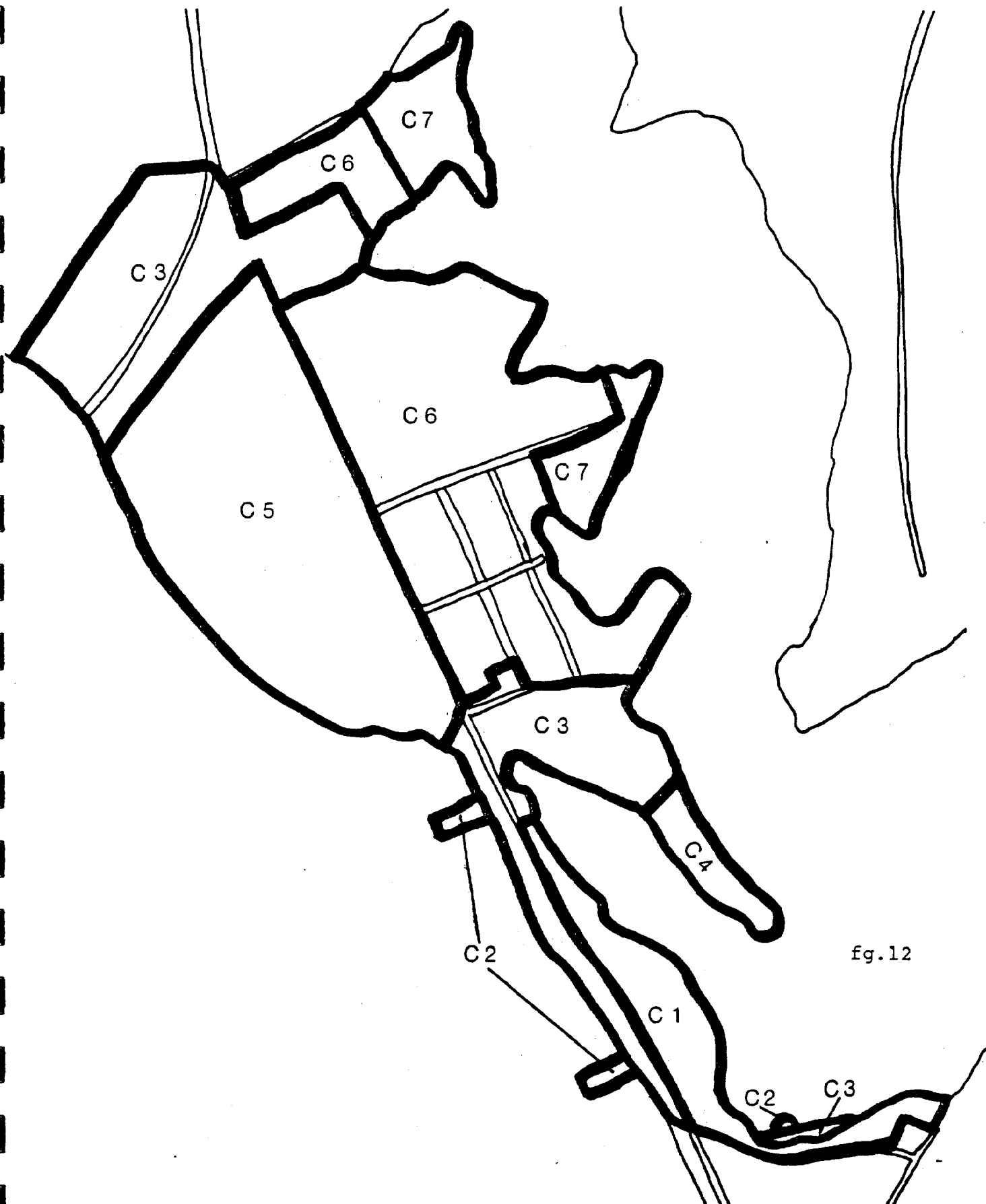
- o The Tiki Bar property and the Harbor Island Marina property between Charles Street, Maltby Street and Patuxent Avenue, Subarea B4, is designated for mixed commercial/residential use. Because the area borders the Sandy Point residential area, it is important to buffer this residential area from commercial incursion. If new residential development were to be proposed, setbacks identified in the zoning ordinance would apply. If new commercial development were to be proposed, a 50 foot no-use setback from adjoining residential lot lines would be required. In addition, fencing and appropriate buffering would be required to reduce the impact on the residential area.
- o Because of the attractiveness of the area and its many water views, it is an area around which many local residents as well as visitors drive. A small turn-around for cars is needed to facilitate the flow of traffic and to prevent sight seekers from encroaching into the residential area. There is a two-stage recommendation to accommodate this need. The first stage would be the carving out, paving and stripping of a small car turn around at the the northern end of Charles Street. Details are provided in Chapter VI. A second stage solution could be the construction and fill of additional land extending out from the current bulkhead at the end of Charles Street. This land could act as a scenic parkland and parking area.
- o A sidewalk is proposed for completion along the southern edge of Charles Street. See Chapter VI, Public Improvements for details.

C. CHARLES STREET TO LORE STREET AREA. (Planning Area C, including Planning Subareas C1,C2,C3,C4,C5,C6,C7)

This area, identified in Figure , the historic commercial core of Solomons Island and Avondale, includes a mix of water-oriented commercial and residential uses between the Narrows and Solomons Island Road; the parking area and bulkhead west of Solomons Island Road; the residential area of Avondale; the marinas on Back Creek and the harbor area; and the open space south of the Johnson Bridge.

1. **OBJECTIVES:** The objectives to be accomplished by the land use recommendations are:

- o To encourage attractive and economically viable commercial and residential uses along Solomons Island Road south of the Island Bridge which support the local community as well as visitors.
- o To recreate a strong sense of the "island" quality of the area, and provide an attractive water-view recreational area for Solomons.
- o To preserve and enhance the residential quality of the Avondale area.
- o To separate the various types of transportation and automotive uses on the Island.
- o To ensure compatibility of adjacent land uses.



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Charles Street to Lore
Planning Area C

2. **CONSIDERATIONS:** The special environmental and land use considerations affecting this area are:

- o Except for the open space south of the Johnson Bridge, Subarea C3 and C5, the majority of this area is fully developed.
- o Because of the condition of the commercial and residential structures south of the Island Bridge, and because of market pressure, rehabilitation or redevelopment is possible.
- o The actual entrance to the Island itself is not obvious, and the appearance of this gateway area is dominated by an expanse of asphalt for parking.
- o The bulkhead area provides the bulk of the parking for tourists coming to Solomons. There have been complaints concerning overnight camping, fishing from the bulkhead, and problems of maintenance and surveillance.
- o In the Narrows and Back Creek area, there is good to excellent water depths for boating.
- o The commercial/marina area along Charles Street is not integrated with the historic commercial core or the Sandy Point area.
- o The shoreline conditions vary considerably for the unprotected shoreline, accretion of sand beaches and erosion of headlands along the Patuxent River and intermittent fringe marshes, and one area of an accreting sand beach on the Back Creek shorelines.
- o There are few, if any, pedestrian amenities in the area.
- o There have been complaints about excessive noise in the commercial area.

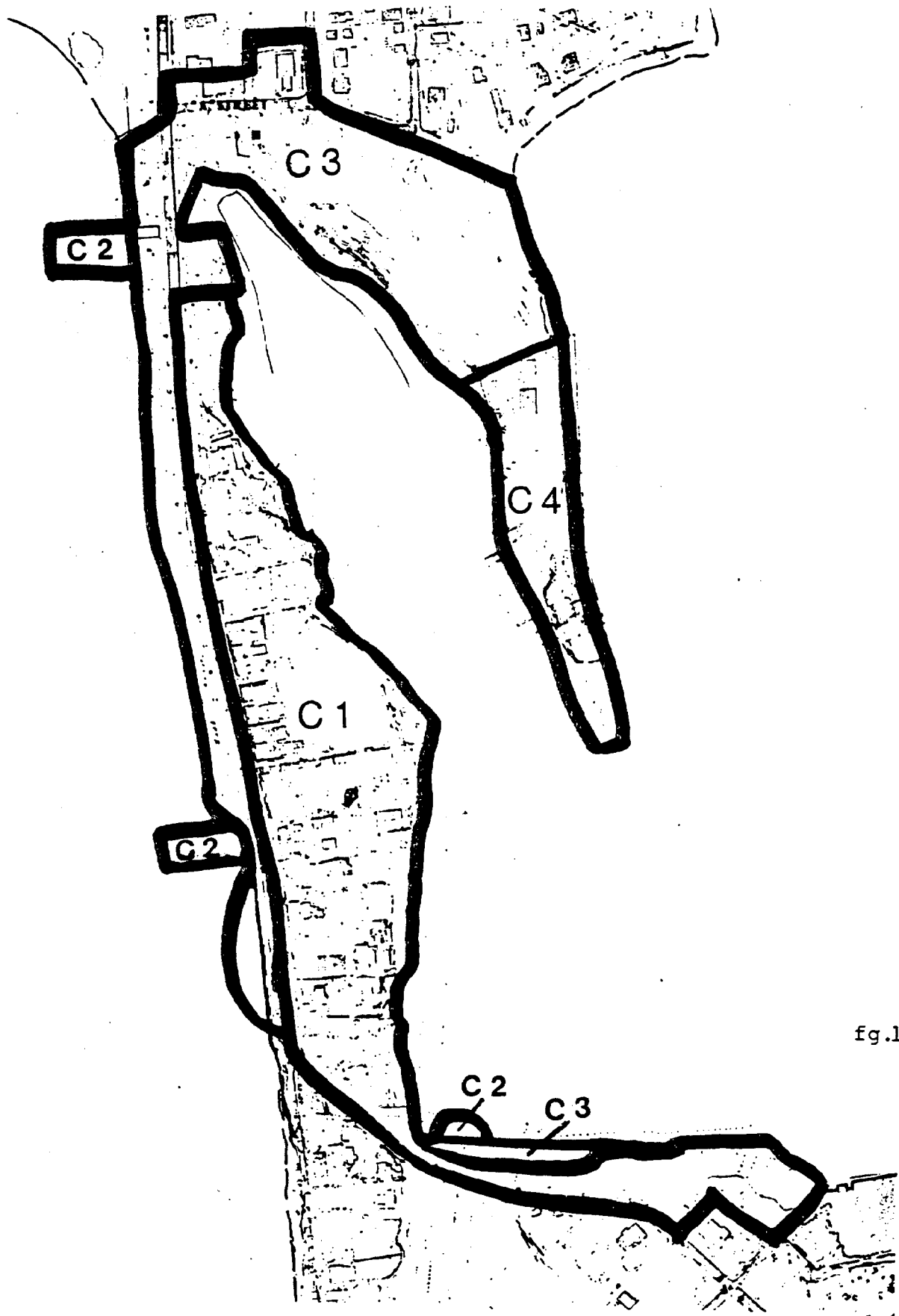


fig.13

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Detail
Planning Area C

3. RECOMMENDATIONS:

- o The property east of Solomons Island Road from the Island Bridge to Charles Street, Subarea C1, is identified for a mix of residential, commercial, and marina use.
- o The Catholic Church property, a segment of Subarea C3, should be designated for institutional use. A scenic easement should be explored for the Church's land on the peninsula at the end of Calvert Street in order to preserve the attractive undeveloped scenic shoreline. Such an easement agreement between the County and the Church would assure that the land would not be visually altered.
- o Residential zoning for Avondale, Subarea C6, should be maintained, with the exception of the existing marina uses, Subarea C7, and Woodburns Grocery which should be designated for commercial use.
- o A architectural and historic survey of structures in this area should be carried out, with the intention of evaluating structures for a historic district designation.
- o The land on the west side of Solomons Island Road south of the Johnson Bridge, Subarea C5, is designated as an Agricultural Conservation District. It must be continued in this use for the period of the conservation agreement (7 years). Except for the land identified for a park and public parking in Chapter V, in Subarea C3, the permitted uses, including a mixture of commercial and residential, are identified in the zoning table for implementation at the end of the conservation agreement period.

- o An improved and expanded bulkhead is proposed for the entrance area to Solomons, Subarea C3. Included in this proposal are: additional parking; a riverwalk, a commercial pier for retail; and new lighting. See Chapter VI, Public Improvements for details.
- o A Recreation Information Center is proposed for the area directly north of the "tidebox" at Solomon's entry. This would provide public restrooms and an information facility. See Chapter VI, Public Improvements for details.
- o A new bulkhead is proposed for the commercial/marina area north of Charles Street. See details in Chapter VI, Public Improvements.
- o A sidewalk is proposed for the eastern side of Solomons Island Road, beginning at Lore Street and continuing down to the corner of Charles Avenue. See details in Chapter VI, Public Improvements.
- o A bikepath is proposed along Solomons Island Road which would provide an alternate means of transportation, and unite the Calvert Marine Museum; the Naval Recreational area; Zahniser's Marina; and the Biological Lab. See details in Chapter VI, Public Improvements.

D. LORE STREET TO DOWELL ROAD. (Planning Area D, including Planning Subareas D1,D2,D3,D4)

The Lore Street to Dowell Road area identified in Figure includes the land north of Lore Street, from the Patuxent Shopping Center east of Route 2/4 north to Dowell Road. It is bounded on the water side by Back Creek.

1. **OBJECTIVES:** The objectives to be accomplished by the land use recommendations are:
 - o To permit commercial and residential development to occur within strict appearance guidelines and shoreline setbacks in order to preserve the scenic and environmental character of the land and the water along Back Creek.
 - o To encourage regional retail and other major commercial development to locate adjacent to Rt. 2/4.
2. **CONSIDERATIONS:** The special environmental and land use considerations affecting this area are:
 - o Land surrounding the Patuxent Shopping Center (Subarea D1) and the area north of New Town Road (Subarea D4) is level and cleared, while much of the rest of the area is densely forested with some residential development on the water and some small scale commercial on Rt 2/4.
 - o This area includes considerable shore areas

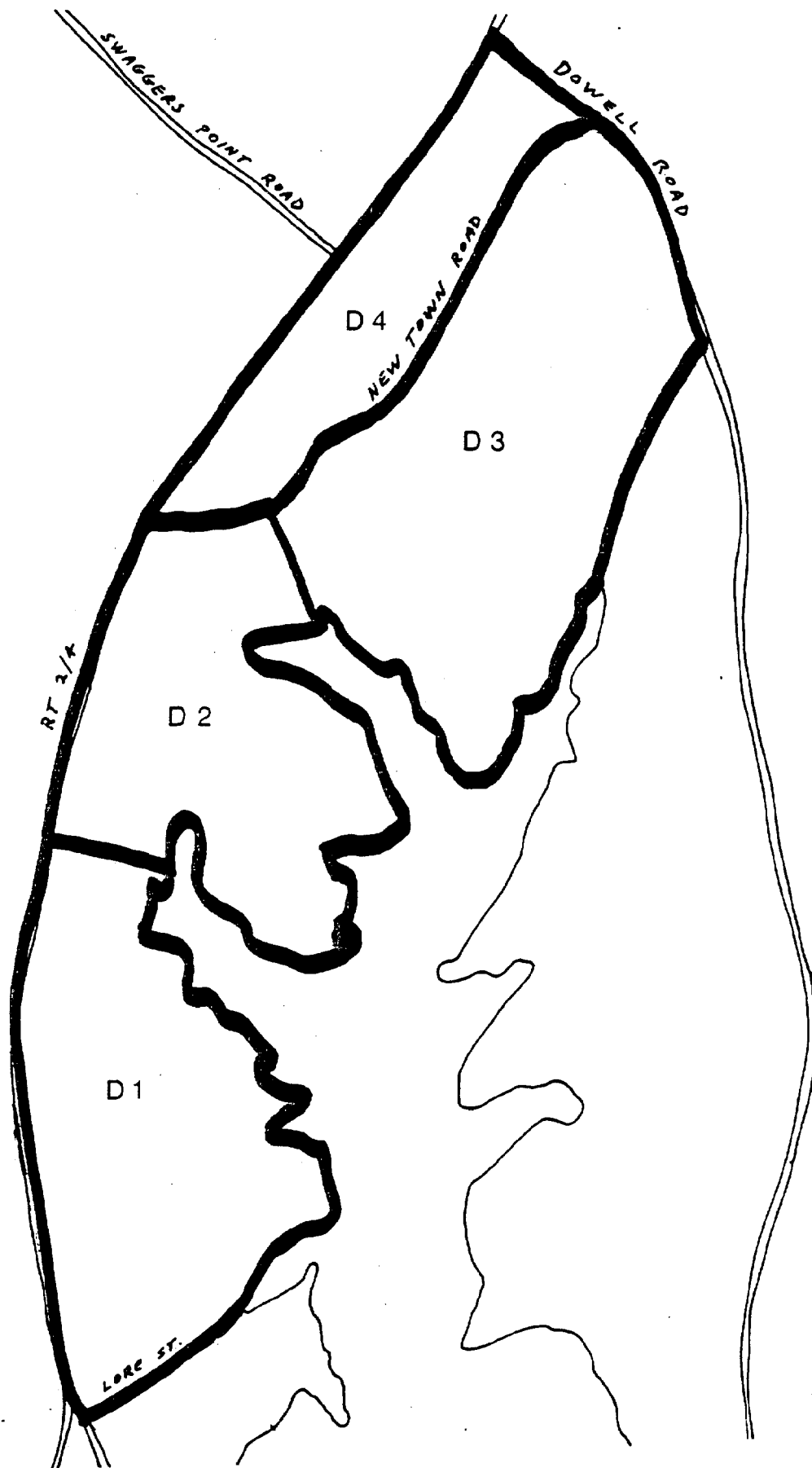


fig. 14

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Lore Street to Dowell Rd
Planning Area D

that are subject to flooding during the estimated 100 year flood event. There are no soils present that are identified as highly erodable soils, but some of the slopes adjacent to the shoreline are steep enough that foot traffic will create erosion problems, and thus care will be needed to protect shoreline soils from excessive use.

- o The shoreline conditions along this reach varies considerably between wetlands, intermittent fringe marshes, bank erosion and bulkheading.
- o The general area is suitable for small craft boating activities, although care must be taken to avoid siting commercial boating facilities in the head waters of Back Creek.
- o Route 2/4 is scheduled to be widened to four lanes in the future and it includes the construction of service roads. This will provide convenient access to regional shopping needs.

3. RECOMMENDATIONS:

- o The land along Route 2/4 in the vicinity of the Patuxent Plaza, Subarea D1, and the area north of New Town Road, Subarea D4 is designated for regional scale commercial use because of its convenient transportation access and because both areas are relatively isolated from residential uses and waterfront areas.
- o The remaining land in this area is designated for residential and/or small scale commercial use. Comprehensive plans for large undeveloped land holdings are encouraged.

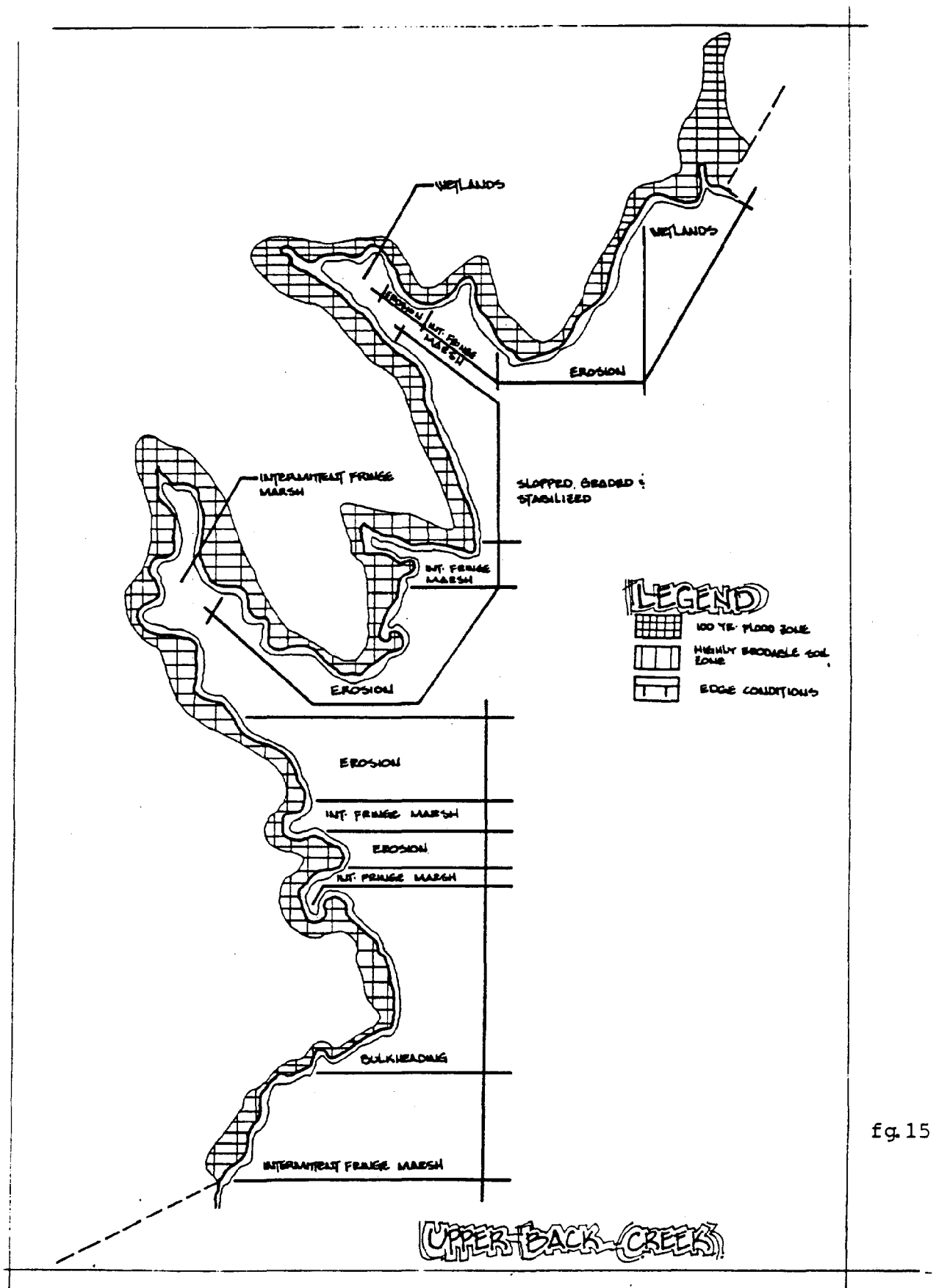


fig. 15

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Shore Profile
Planning Area D

E. DOWELL PENINSULA AREA. (Planning Area E, including Subareas E1, E2)

The Dowell Peninsula area identified in Figure includes the land within the Town Center which is located on the Dowell Peninsula. Calvert Marina owns most of the undeveloped land at the southern end of Dowell Road in Subarea E1. The rest of the area, Subarea E2, to the north is developed in individual single family homes.

1. **OBJECTIVES:** The objectives to be accomplished by the land use recommendations are:
 - o To permit and encourage an attractive mix of residential, marina, and commercial uses to be developed on the Calvert Marina site.
 - o To preserve and protect the natural water environment along the Dowell Peninsula while permitting carefully planned residential and commercial development.
2. **CONSIDERATIONS:** The special environmental and land use considerations affecting this area are:
 - o There is a large, single-owner, undeveloped land area on the southern portion of the peninsula, Subarea E1, where large scale marina boat usage has been established.



fig.16

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Dowell Peninsula
Planning Area E

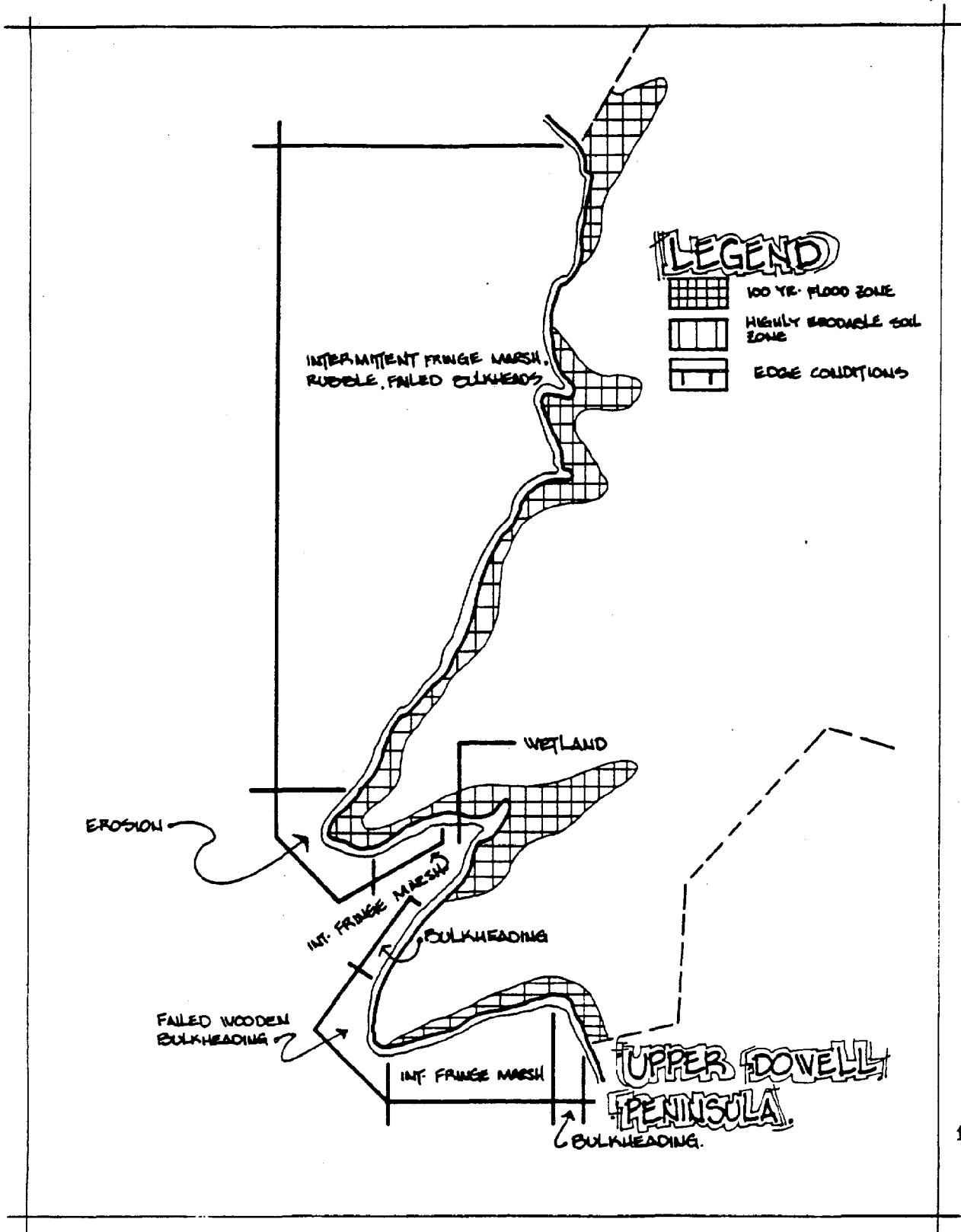


fig 17

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Shore Profile
Planning Area E

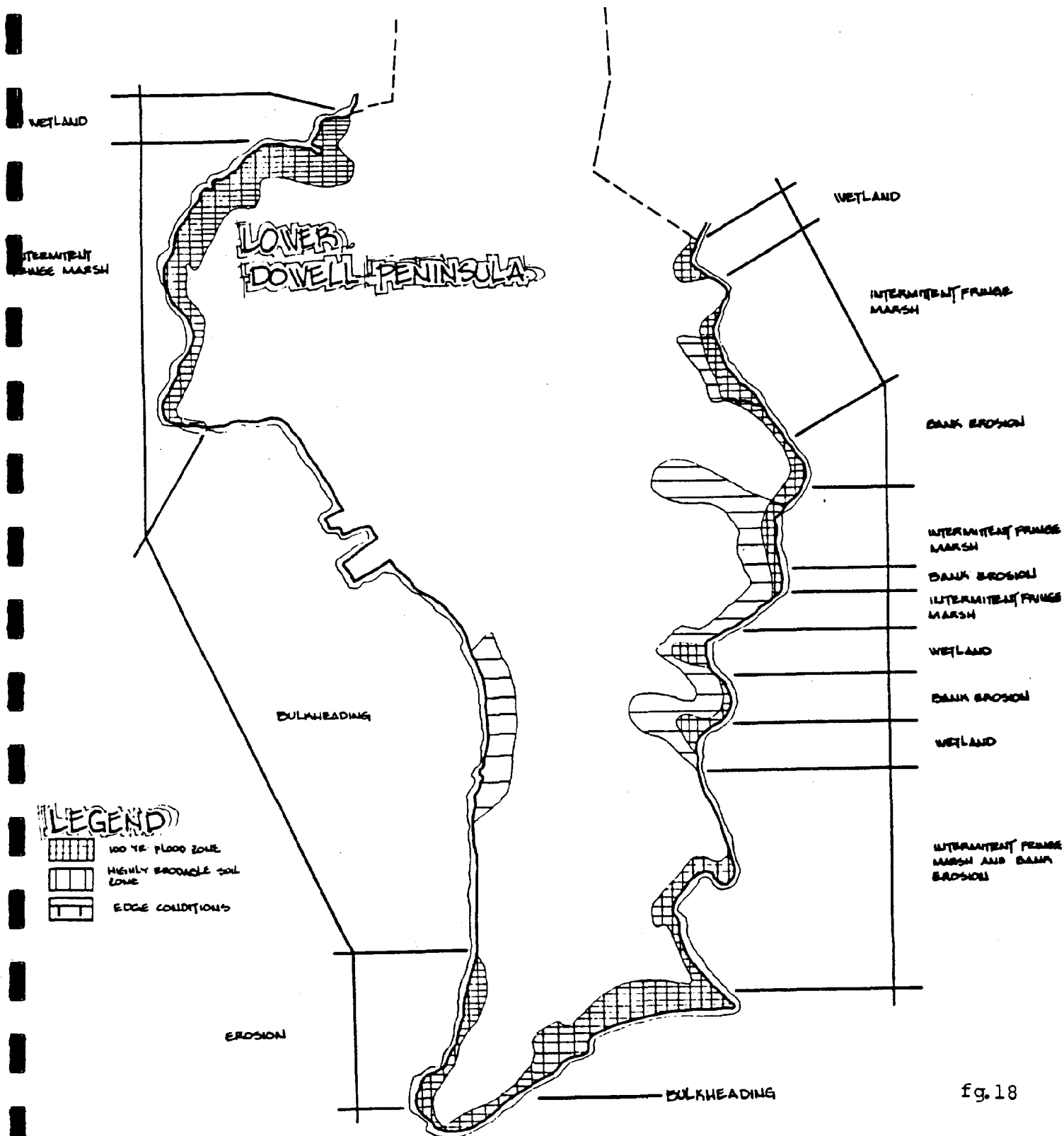


fig.18

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Shore Profile
Planning Area E

- o There are many small parcels with residential units on them in the northern part of the peninsula, Subarea E2.
- o There is considerable variation in types and locations of environmental constraints due to the size of the area.
- o The shoreline conditions vary between existing bulkheading in good repair, failing bulkheading, intermittent fringe marsh, wetlands, and bank erosion.
- o The steepness of the shoreline in the northern portion of this area will require controlled foot access if erosion problems are to be avoided.
- o The relatively deep water associated with the southern half of the peninsula is highly suited for small craft boating use; however, the northern half of the peninsula is associated with relatively shallow water and is less desirable for commercial small craft activities.
- o Large scale development in the southern portion of the peninsula might generate substantial traffic along Dowell Road.

3. RECOMMENDATIONS:

- o The Calvert Marina property is designated for mixed residential, marina and commercial use. Comprehensive plans for the entire Subarea E1 are encouraged. It will be extremely important to abide by appearance and landscape guidelines because of the potential visual impact of development on the site.

- o The other land on the Dowell Peninsula is designated for residential and local serving commercial uses.
- o A traffic analysis of Dowell Road should be done to assess future road improvement needs.

F. SWAGGERS POINT AREA. (Planning Area F, including Planning Subareas F1, F2)

The Swaggers Point Area identified in Figure includes the single family residential area south of Swaggers Point Road, Subarea F1, and the largely undeveloped land west of Rt 2/4 and north of the Navy Recreation Center, Subarea F2.

1. **OBJECTIVES:** The objectives to be accomplished by the land use recommendations are:
 - o To preserve and enhance the residential quality of the property along Swaggers Point Road.
 - o To permit and encourage an attractive mix of residential and small scale commercial uses in Subarea F1.
2. **CONSIDERATIONS:** The special environmental and land use considerations affecting this area are:
 - o The property along Swaggers Point Road is currently developed with large lot single family homes.
 - o There are small residential structures and

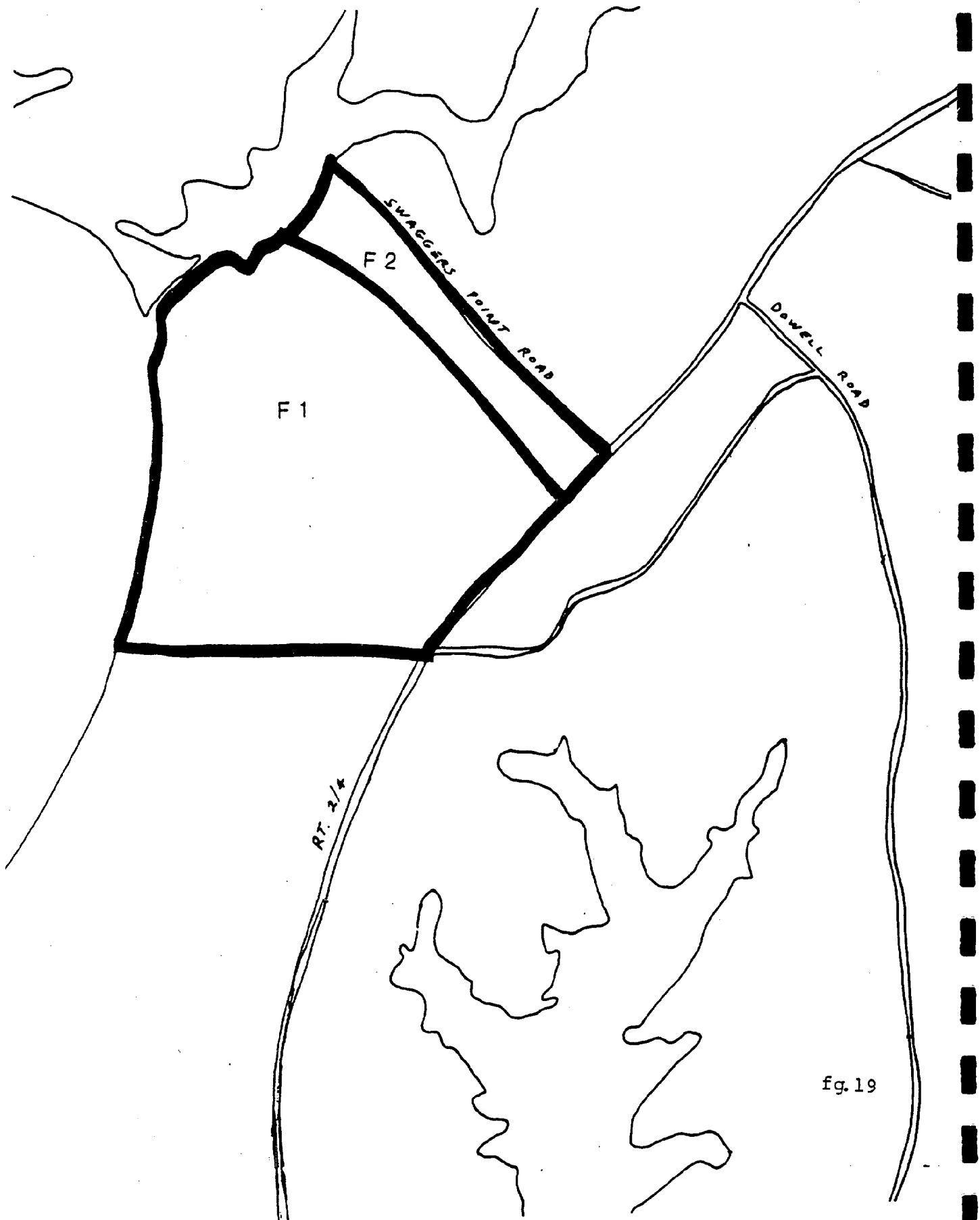





fig. 19

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Swagers Point
Planning Area F

LEGEND

-  100 YR. FLOOD ZONE
-  HIGHLY ERODIBLE SOIL ZONE
-  EDGE CONDITIONS

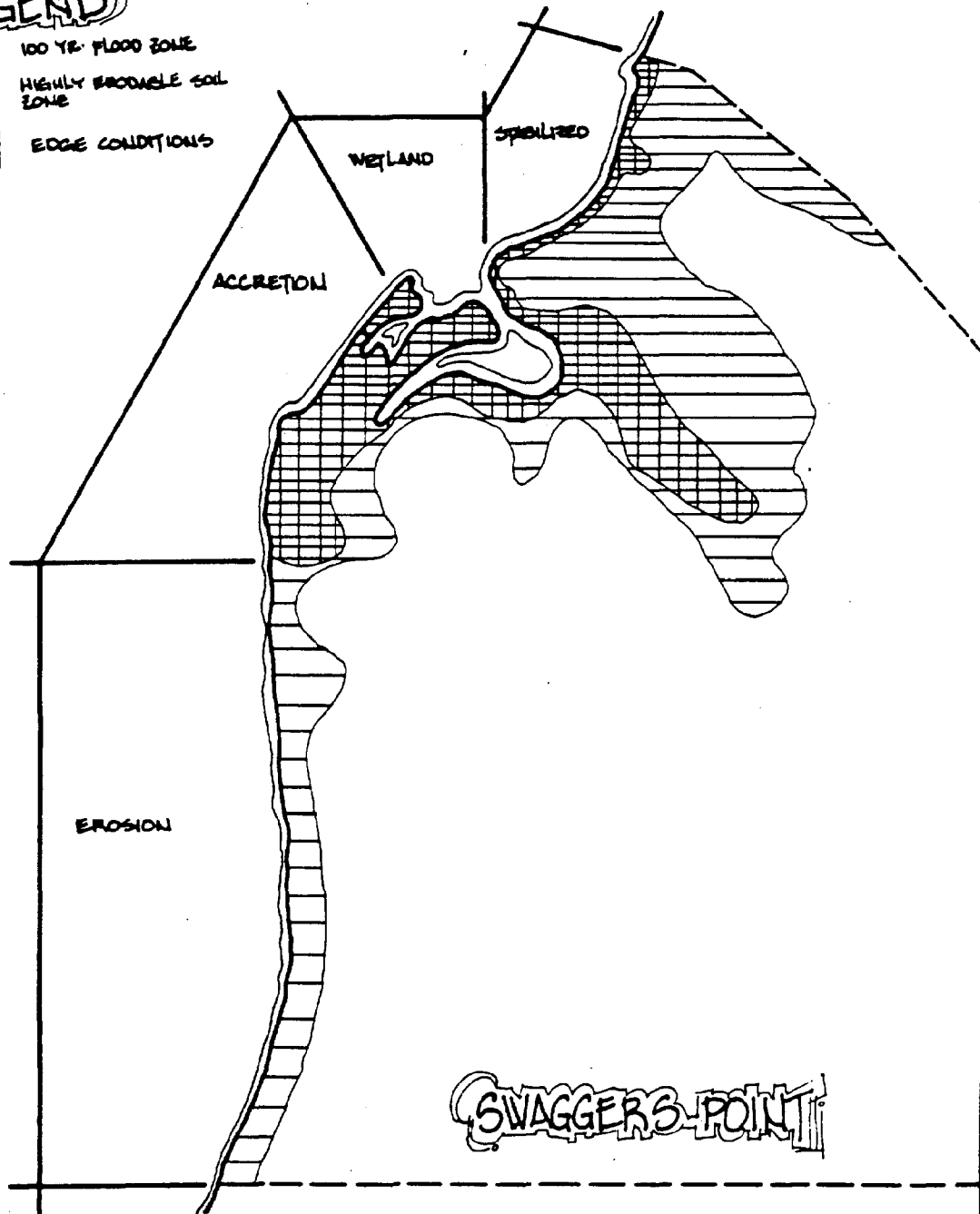


fig. 20

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Shore Profile
Planning Area F

a commercial business along Rt 2/4.

- o Most of the rest of Subarea F1 is in agricultural use except for a portion of the property which is being used as ball field.
- o The western side of the planning area is characterized by high bluffs of about 30 feet overlooking the Patuxent River which are steep and have unstable soils and high erosion rates.
- o The only portion of the area that is within the flood hazard boundary is the shoreline area associated with the small creek in the northwest portion of Subarea F1.

3. **RECOMMENDATIONS:**

- o The property along Swaggers Point Road should remain in residential use.
- o The land in Subarea F1 is designated for residential and/or convenience commercial use. Comprehensive plans for large undeveloped land holdings are encouraged.

G. **ST. JOHNS CREEK AREA.** (Planning Area G)

The St. Johns Creek area is identified in Figure and includes the land within the Town Center that is north of Dowell Road and east of Rt 2/4.

1. **OBJECTIVES:** The objectives to be accomplished by the land use recommendations are:

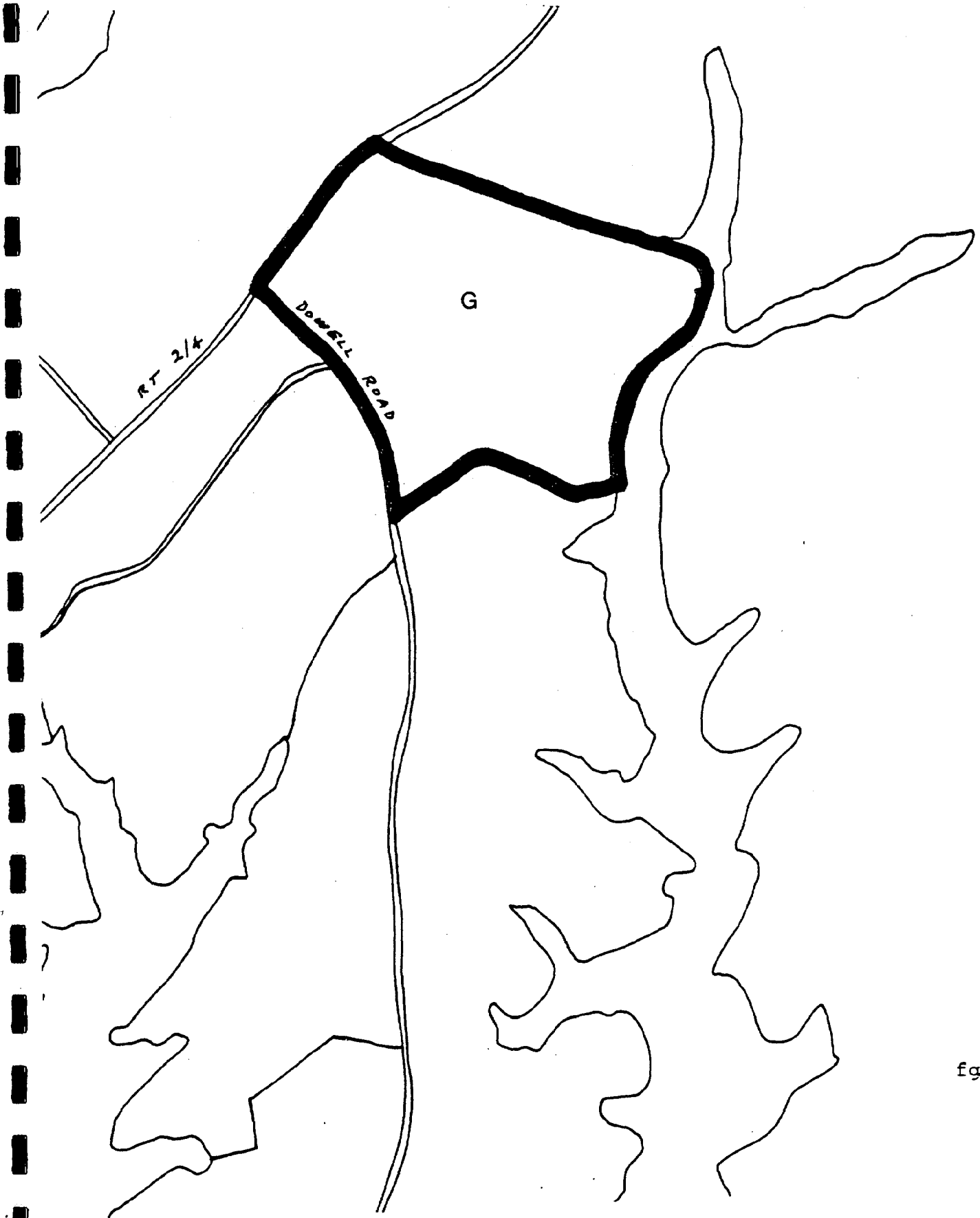
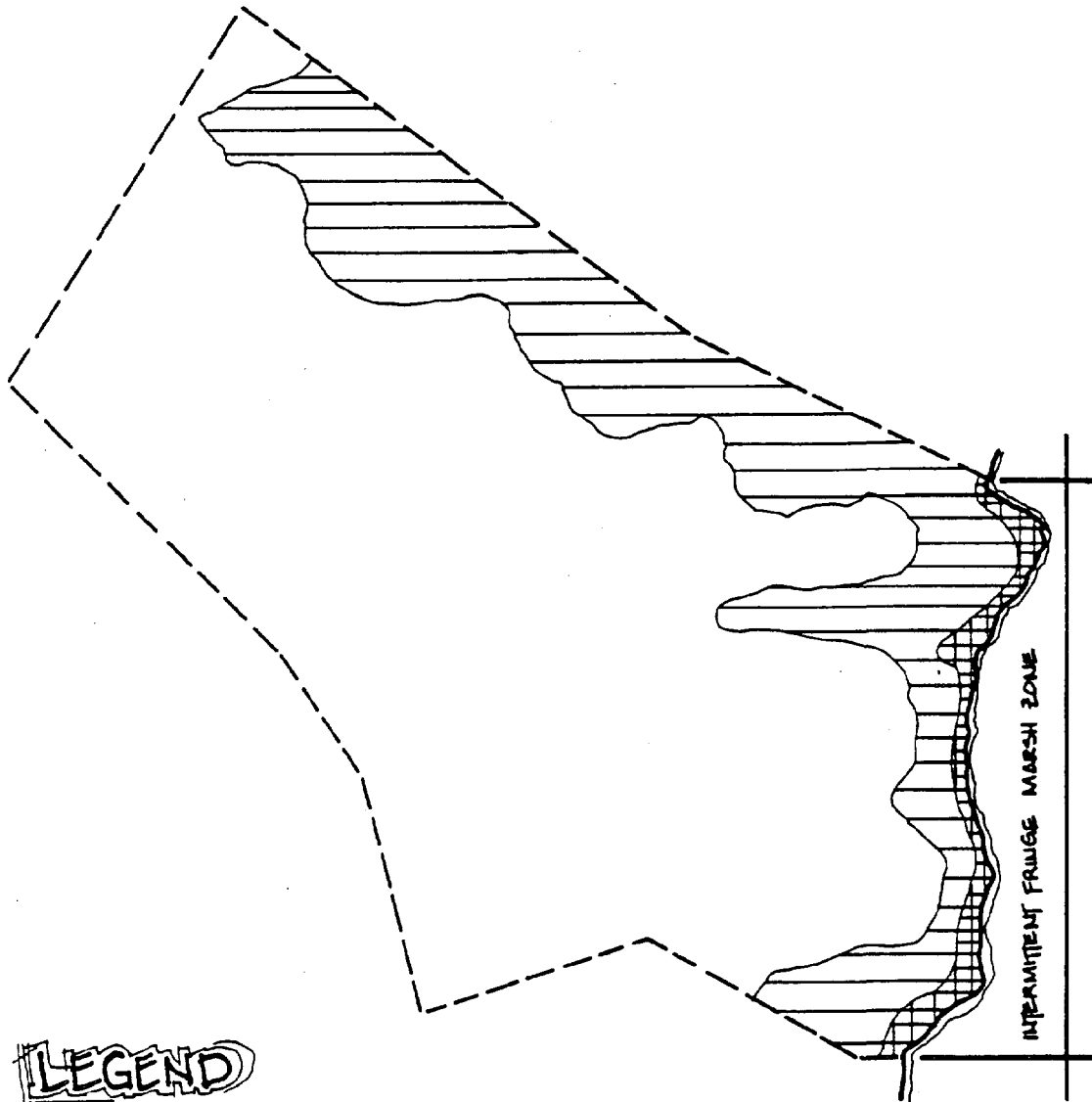


fig 21

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St. Johns Creek
Planning Area G



LEGEND

- 100 YR. FLOOD ZONE
- HIGHLY ERODIBLE SOIL ZONE
- EDGE CONDITIONS

ST. JOHN'S CREEK

fig 22

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Shore Profile
Planning Area G

- o To permit and encourage an attractive mix of residential and commercial uses to be developed in the area.
- o To preserve and protect the natural environment of St. Johns Creek.

2. **CONSIDERATIONS:** The special environmental and land use considerations affecting this area are:

- o The land is basically undeveloped except for a residence in the southern portion of the area near St Johns Creek and some commercial use along Rt 2/4.
- o There is little potential for significant boating activity due to the shallow water and narrowness of St. Johns Creek in the area.
- o The area has a large amount of interior and waterfront land that is considered unstable soil and subject to high erosion.
- o The bluffs along St. Johns Creek are of moderate height.
- o The shoreline conditions are intermittent fringe marshes and heavily wooded.

3. **RECOMMENDATIONS:**

- o The area is designated for residential and/or commercial use. Comprehensive plans for large undeveloped land holdings are encouraged.

- o Water-use related activity should be discouraged along St. Johns Creek because of the shallowness and sensitive shoreline conditions.

CHAPTER III

PERMITTED LAND USES AND REQUIREMENTS

CHAPTER III

PERMITTED LAND USES AND REQUIREMENTS

- A. Introduction**
- B. Permitted Land Uses**
- C. Conditional Uses**
- D. Special Development Requirements**
 - 1. Lot Area, Lot Width & Setback Requirements for Residential Development
 - 2. Lot Area, Lot Width & Setback Requirements for Commercial Development and for Residential Development adjacent to Commercial Uses
 - 3. Lot Area, Lot Width & Setback Requirements for Industrial Development
 - 4. Height
 - 5. Special Height and Setback Requirements for Waterfront Development
 - 6. Signs
 - 7. Parking and Loading Requirements
 - 8. Marine Facilities Regulations
 - 9. Site Plan Review

E. Overlay Districts

1. Historic Districts
2. Agricultural Preservation Districts
3. Business Directional Sign Districts

A. INTRODUCTION.

This chapter specifies the permitted land use for each of the planning subareas, describes additional conditional use requirements, lists special development requirements and describes overlay district requirements that apply to the Solomons Town Center.

B. PERMITTED LAND USES.

Table 1 is a list of Permitted Land Uses by Planning Subarea. The Table lists the uses permitted and the Geographic Subareas in the Solomons Town Center in which they are permitted. No use shall be allowed except as indicated in the Table.

Following is a key to the Table:

- P - Permitted Use. Uses designated by the letter "P" shall be permitted on any lot in the geographic areas indicated, subject to all applicable regulations.
- S - Special Exception. Uses designated by the letter "S" may be authorized by the Board of Appeals in accordance with the provisions of Article 7-3.01 C of the Zoning Ordinance.
- Sl - Permitted Use, subject to a Special Exception if less than 150' from a residential building or site where there is an active house permit

TABLE 1

PERMITTED LAND USES BY PLANNING SUBAREA

	B1	B2	B3	B4	C1	C2	C3	C4	C5	C6	C7	D1	D2	D3	D4	E1	E2	F1	F2	G
AGRICULTURAL																				
Field Crops and/or Nursery									P			P	P	P	P	P	P	P	P	P
Forestry or Tree Farming												P	P	P	P	P	P	P	P	P
Commercial Greenhouse												P	P	P	P	P	P	P	P	P
Domestic Animals	P	P	P	P	P			P	P	P	P	P	P	P	P	P	P	P	P	P
Livestock									C			C	C	C	C	C	C	C	C	C
Commercial Raising of Exotic Animals									S			S	S	S	S	S	S	S	S	S
Veterinary Hospital or Clinic									P			C								
Farm Building									P			P					P	P	P	P
RESIDENTIAL																				
Single Family Dwelling Detached	P	P	P	P	P			P	P	P	P	P	P	P	P	P	P	P	P	P
Attached Dwellings:																				
Duplex, Triplex and Fourplex			P	P	P			P	P*	P	P	P	P	P	P	P	P	P	P	P
Townhouse			P	P	P			P	P*	P	P	P	P	P	P	P	P	P	P	P
Multi Family Dwelling			S1	S1	S1			C	C	C	C	S1	S1	S1	S1	P	C	S1	S1	S1
Accessory Apartment			C	C	C			C	C	C	C	C	C	C	C	C	C	C	C	C
Bed and Breakfast Facility:																				
Two Bedrooms in Use	C	C	C	C	C			C	C	C	C	C	C	C	C	C	C	C	C	C
Three to Five Bedrooms in Use	SC	SC	SC	SC	SC			SC	SC	SC	SC	SC	SC	SC	SC	SC	SC	SC	SC	SC
Boarders in Residence (no more than 3)	P	P	P	P	P			P	P	P	P	P	P	P	P	P	P	P	P	P
Boarding House			C	C	C			C	C	C	C	C	C	C	C	C	C	C	C	C
Group Home	P	P	P	P	P			P	P	P	P	P	P	P	P	P	P	P	P	P
Tenant House									C			C	C	C	C	C	C	C	C	C
Emergency Mobile Home	C	C	C	C	C			C	C	C	C	C	C	C	C	C	C	C	C	C

TABLE 1
(continued)

PERMITTED LAND USES BY PLANNING SUBAREA

COMMERCIAL RETAIL	B1	B2	B3	B4	C1	C2	C3	C4	C5	C6	C7	D1	D2	D3	D4	E1	E2	F1	F2	G
Agricultural Machinery, Service and/or Supplies															P					
Antique Sales	C	C	C	C	P	P			C*	C	P	P	C		P	C	C	C	C	C
Auction Building															P					
Boat Dealer					S2			S2			S2	S2			S2	S2				
Itinerant Sales:																				
Fish, Shellfish, Fruit, Vegetables					C		C					C			C					
All Other Licensed Sales												C			C					
Roadside Stand									C			C	C	C	C	C	C	C	C	C
Farmers' Market						P														
Lumber and/or Other Building Materials					S2							P			P					
Other Commercial Retail:						P	P	P	P*	C	P	P	P	P	P	P	S	P	P	P
Less than 2,500 square feet																				
More than 2,500 square feet																				

COMMERCIAL BUSINESS AND PERSONAL SVCS	B1	B2	B3	B4	C1	C2	C3	C4	C5	C6	C7	D1	D2	D3	D4	E1	E2	F1	F2	G
Commercial Trade/Business School Ofcs.																				
Boat Service and/or Repair					S			S	S			S1			P					
Boat Storage					C			C	C						P	S				
Commercial Boat Dock					P			P	P							P				
Eating Establishments:																				
Restaurant																				
Drive-in, fast food, carry-out									P*		P	P	P	P	P	P	P	P	P	P
Funeral Service and/or Crematorium												S2			P	S2				
Home Occupations:												P								
All Employed are Residents																				
1 Equiv. Full-time Non-resident Empl.									C	C	C	C	C	C	C	C	C	C	C	C
2 Equiv. Full-time Non-resident Empl.									SC	SC	SC	SC	SC	SC	SC	SC	SC	SC	SC	SC
Laundry/Laundromat																				
Motel or Hotel																				
Tavern, Nightclub, Lounge, Dance Hall																				
Other Commercial Businesses, Personal Services, Offices, Clinics:																				
Less than 2,500 square feet																				
More than 2,500 square feet																				

TABLE 1
(continued)

PERMITTED LAND USES BY PLANNING SUBAREA

	B1	B2	B3	B4	C1	C2	C3	C4	C5	C6	C7	D1	D2	D3	D4	E1	E2	F1	F2	G
COMMERCIAL RECREATION																				
Carnival, Fair or Circus - Temporary:																				
Less than Five Acres									C			C			C			C		C
More than Five Acres									SC			SC			SC			SC		SC
Convention Center/Retreat									P*			P			P			P		P
Golf, Miniature												S			S					
Indoor Commercial Amusements:																				
Arcade, Pool Hall, Bingo												S			S					
Bowling, Skating Rink, Theatre												P			P					
Dance Studio				P	P							P			P					
Health Spa/Gymnasium				P	P			P	P*		P	P	P		P			P		P
Swimming Pool, Athletic Court				Sl	Sl				P*			P	P		P			P		P
Marina or Yacht Club				S	S			S			S	S			S					
Boatel				C	C			C	C*		C	C	C		C					
Other Outdoor Recreation, such as:																				
Swimming Pools, Athletic Courts				Sl	Sl	Sl		Sl	Sl*		Sl	Sl	Sl		Sl			Sl		Sl
COMMERCIAL WHOLESALE																				
Wholesaling or Warehouse:																				
Indoor												S			S			S		S
Mini-storage												S			S			S		S
MOTOR VEHICLE & RELATED SERVICES																				
Automobile Filling Station, Repair Shop																				
Automobile Parking Lot/Garage												SC			SC					SC
Commuter Parking Lot																				
Motor Vehicle Dealer - New or Used												P	P	P	P			P		
Other Motor Vehicle Related Uses incl:												S2			S2					S2
bus depot, car wash, taxi service,												S2			S2					S2
vehicle rental or leasing																				

TABLE 1
(continued)

PERMITTED LAND USES BY PLANNING SUBAREA

INDUSTRIAL	B1	B2	B3	B4	C1	C2	C3	C4	C5	C6	C7	D1	D2	D3	D4	E1	E2	F1	F2	G
Research & Development Facility:																				
Environmental																		P	P	
Other																		P	P	
Watercraft Manf. and/or Assembling																S				

Research & Development Facility:

Environmental

Other

Watercraft Manf. and/or Assembling

INSTITUTIONAL

Child Day Care Center:

12 Children or Less

13 Children or More

College or University

Fire and/or Rescue Service

Museum or Library

Non-profit Club or Institution

Nursing or Convalescent Home

Place of Worship, Parish Hall, Convent,

Monastery or Rectory

Public or Governmental Building

Public or Non-Profit Park and/or

Recreation Area, including:

Tennis Courts, Swimming Pools,

Athletic Fields, etc.

UNCLASSIFIED

Accessory Building or Use

Cemetery or Memorial Garden

Dock, Pier, Private

Flea Market by Non-Profit Organization

Garage Sale, Yard Sale or Estate Sale

Heliport

Inoperative Vehicle, one per lot

Outdoor Storage in Connection with

Commercial and/or Industrial Uses

Model Home - No Office

Subdivision Entrance

Temporary Structure Incidental to

Construction (non-residential)

or an established Historic District.

- S2 - Permitted Use, subject to a Special Exception if less than 300' from a residential building or site where there is an active house permit or an established Historic District.
- C - Permitted Use, subject to certain conditions. The conditions are listed in Article 3-3 of the Zoning Ordinance and paragraph C of this Chapter.
- SC - Permitted Use, subject to a Special Exception if it meets conditions.
- * - Uses not allowed in the Agricultural Conservation District until expiration of the agreement.

C. CONDITIONAL USES.

The following are conditions imposed upon land uses indicated in Table 1, Permitted Land Uses by Geographic Subarea. These conditions are in addition to the conditions listed in Article 3-3 of the Zoning Ordinance. (See Appendix A for a copy of the Conditional Use requirements from the Zoning Ordinance.)

1. Antique Sales, provided that:
 - a. it is a home occupation,
 - b. all sales or display merchandise are inside an enclosed structure,
 - c. adequate off-street parking is available, but does not exceed four (4) parking spaces,
 - d. signs indicating the business name do not exceed four (4) square feet in size, and
 - e. applicable State Licenses are obtained.

2. Itinerant Sales: Fish, Shellfish, Fruit, Vegetables,
provided that:
 - a. sales are made from a boat on the water in Subarea C3.
3. Other Commercial Retail, provided that:
 - a. the business was in existence at the time of adoption of this Plan.
4. Boat Storage, provided that:
 - a. no multi-level boat storage is permitted.
5. Eating Establishments: Restaurant, provided that:
 - a. the location of the restaurant is in an historic district.
6. Tavern, Nightclub, Lounge, Dance Hall, provided that:
 - a. there are adequate safeguards to protect against noise levels that would exceed state standards.
7. Boatel, provided that:
 - a. no boat servicing or repair is permitted.

D. SPECIAL DEVELOPMENT REQUIREMENTS

There are certain special development requirements that

apply to new development or substantial rehabilitation projects that occur on lands within the Solomons Town Center. These are requirements that are more general and do not apply to the specific planning areas identified above.

The objectives of these special development requirements are:

- o To encourage new attractive residential and commercial development to occur within the Solomons Town Center.
- o To provide for efficient uses of land, environmental safeguards, and high quality site design through the use of flexible development controls.

Based on these objectives, the following are special development requirements that apply in the Solomons Town Center:

1. Lot Area, Lot Width & Setback Requirements for Residential Development

The provisions of Articles 5-1.02 and 5-1.03 in the Zoning Ordinance apply only if all adjacent lots are in residential use and/or are planned for residential use in an approved site plan. Otherwise, the provisions of paragraph D2 below apply.

2. Lot Area, Lot Width & Setback Requirements for Commercial Development and for Residential Development adjacent to Commercial Uses

a. Minimum Lot Area

There is no minimum lot area for commercial use. The minimum lot area for residential use must be as specified in Articles 5-1.02 and 5-1.03 of the Zoning Ordinance. For development to occur, all lots must meet the definition of a "buildable lot" as defined in the Zoning Ordinance.

b. Minimum Depth of Front Yard

In Planning Areas B and C, the minimum depth of the front yard is 10 feet from the right-of-way. Otherwise, the minimum depth of the front yard is 100 feet from Minor Arterial roads (Rt 2/4); 50 feet from Collector Roads (Dowell Road and future access roads for Rt 2/4); and 10 feet from all other roads.

c. Minimum Width of Each Rear and Side Yard

The minimum width of each rear and side yard is as follows:

- (1) Between residential and commercial uses - 50 feet from adjoining parcels.
- (2) Between commercial uses - 30 feet from adjoining parcels. This may be reduced to zero feet if:
 - o emergency vehicle access and maintenance are addressed,
 - o approval is given by the Fire Marshal,
 - o the adjoining property has a compatible use, and
 - o permission is received from the adjoining property owner.
- (3) In Planning Subarea C1 only, between residential and commercial uses - 6 feet.
- (4) In Planning Subarea C1 only, between commercial uses - 6 feet. This may be reduced to zero feet if:
 - o emergency vehicle access and maintenance are addressed,
 - o approval is given by the Fire Marshal,

- o the adjoining property has a compatible use, and
 - o permission is received from the adjoining property owner.
- (5) In Planning Subareas B1, B3, C1, C3, C4, C6 and C7 only, the rear setback from the mean high water line is 20 feet. This may be reduced in Planning Subareas B3 and C1 to 10 feet if the bulkhead is stabilized and an easement is provided to the County for future boardwalk construction.
- (6) In Planning Subarea C2, a zero foot side and rear yard width is permitted.

3. Lot Area, Lot Width & Setback Requirements for Industrial Development

The provisions of Article 5-1.04 in the Zoning Ordinance apply.

4. Height

- a. In Planning Areas B and C, height of structures is limited to 2 stories plus a habitable roof, but not to exceed 35 feet.
- b. In Planning Areas D, E, F and G, height of structures is limited to 3 stories plus a habitable roof, but not to exceed 50 feet. See paragraph D5 below which sets further requirements for waterfront development.
- c. Steeples, flag poles, masts, transmission line poles and towers, water tanks, monuments and similar structures are exempt from the height restrictions.



fig 23

SOLOMONS MASTER PLAN

PREPARED FOR CALVERT COUNTY BY THE PHIPPS GROUP, J.M. HUTTO ASSOCIATES AND ALAN SPARBER & ASSOCIATES

Height Limit
Planning Areas B&C

5. Special Height and Setback Requirements for Waterfront Development

These height and setback requirements are designed to preserve the historic skyline, waterfront environment and small town character of the Solomons Town Center. Within Planning Areas D, E, F and G, the following shoreline height and setback requirements apply (See Figure 24):

- a. No development may occur within the first 100 feet of shoreline, measured from the mean high water mark.
- b. Between 100 and 150 feet from the mean high water mark, there is a height limitation of 35 feet.
- c. After 150 feet, the height limitation is 50 feet.

6. Signs

The provisions of Article 5-2 in the Zoning Ordinance apply.

7. Parking and Loading Requirements

The provisions of Article 5-3 in the Zoning Ordinance apply.

8. Marine Facilities Regulations

The provisions of Article 5-4 in the Zoning Ordinance as well as the Waterfront Development Guidelines in Chapter IV apply to marine facilities development.

MAXIMUM
HEIGHT

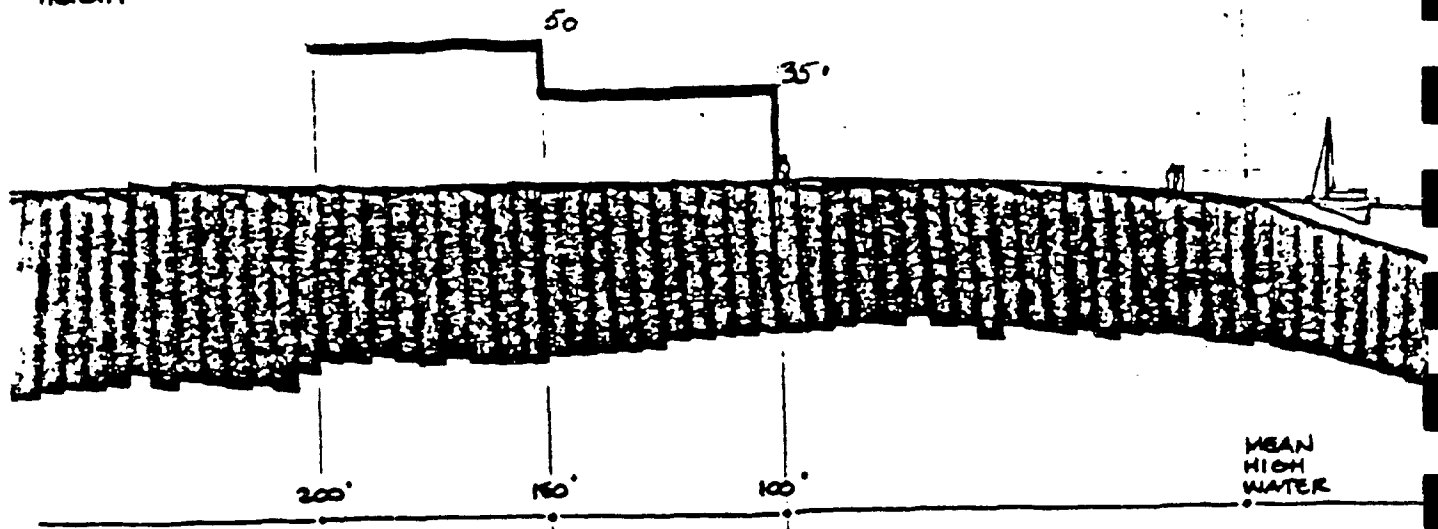


fig.24

SOLOMONS MASTER PLAN

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Waterfront
Height Setbacks

9. Site Plan Review

All new development and substantial rehabilitation of structures in the Solomons Town Center require a Site Plan Review. Substantial rehabilitation shall mean an alteration of an existing structure which increases the gross floor area by at least 50%. The Site Plan Review shall be accomplished in accordance with Article 6 of the Zoning Ordinance.

E. OVERLAY DISTRICTS

According to the Zoning Ordinance, "Overlay Districts are created for the purpose of imposing special regulations in designated areas of the County to accomplish the purposes set forth for each overlay district."¹

1. Historic Districts

It is recommended that the Historic District Commission survey structures within the Solomons Town Center and determine which structures have historic or architectural significance. Those structures which are important should be protected in an Historic District.

2. Agricultural Preservation Districts

The principal purpose of the district is to preserve prime agricultural land and to guide development away from prime agricultural land.² Most of Subarea C5 is currently included in an Agricultural Preservation District (APD). Because the purpose of the Town Center is to be a focal point in the County for growth, it is recommended that no additional APD's be designated in the Solomons Town Center.

¹Article 4-0.01, Zoning Ordinance.

²Article 4-2.01, Zoning Ordinance.

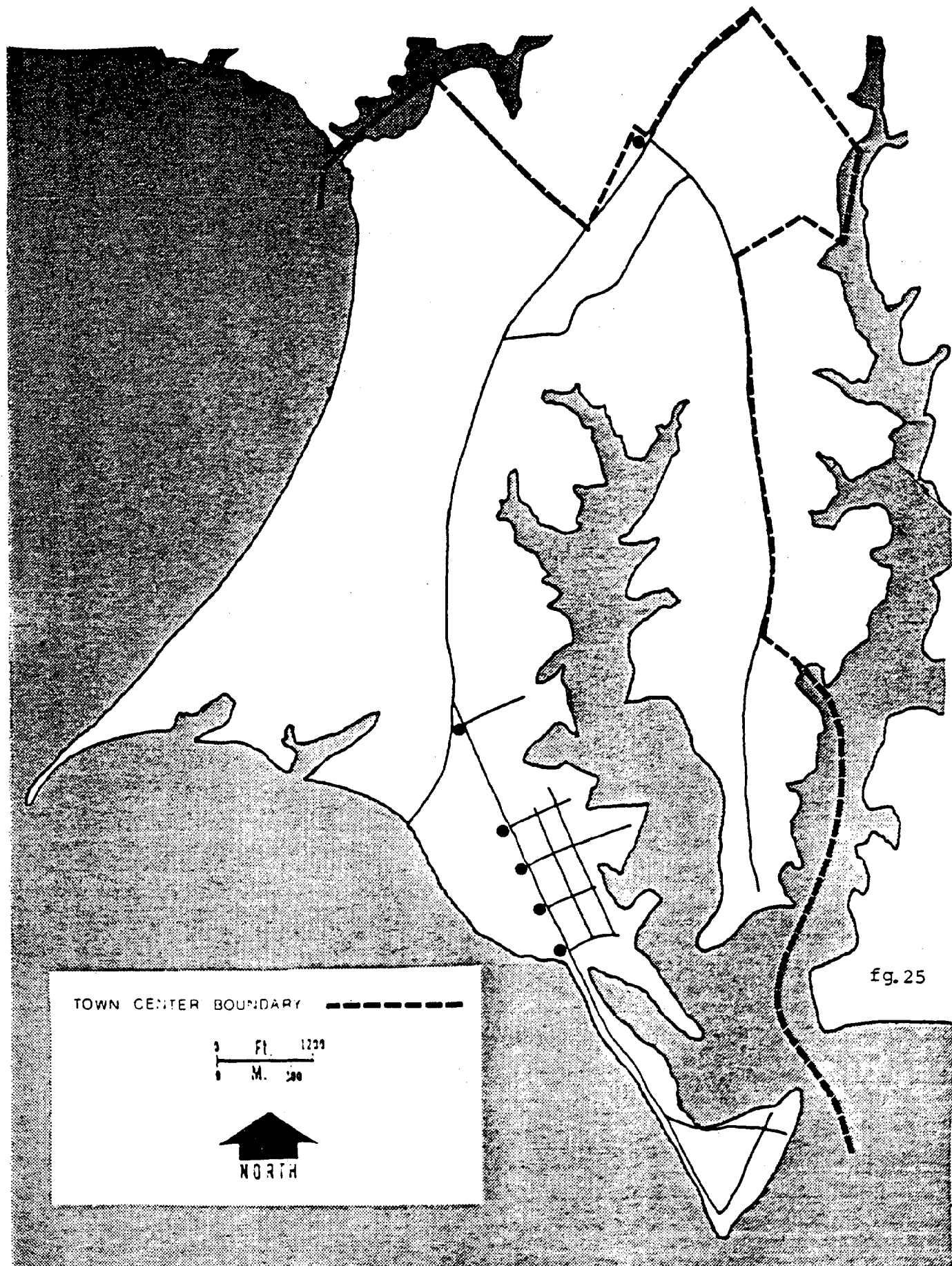
3. Business Directional Sign Districts

According to the Zoning Ordinance, "the purpose of this District shall be to provide space for businesses to advertise their locations and to provide space for Town Center entrance signs."³

It is recommended that a district be formed in the Solomons Town Center to include land within 10 feet of both sides of Route 2/4 and Solomons Island Road. It is further recommended that:

- o Solomons Town Center entrance signs be installed at the northern entrance to the Town and at the foot of the Johnson Bridge ramp.
- o Business Directional Signs be installed to identify businesses or institutions which are off Route 2/4 or Solomons Island Road. Recommended sign locations are given in Figure 25.
- o Business Directional Signs be installed and managed by the County. A uniform sign design similar to Figure 26 be adopted.
- o A schedule of fees be developed for businesses that wish to lease sign space.

³Article 4-5.01, Zoning Ordinance.



TOWN CENTER BOUNDARY - - - - -

0 Ft. 1200
0 M. 200



fig. 25

SOLOMONS MASTER PLAN

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Location of
Business Signs

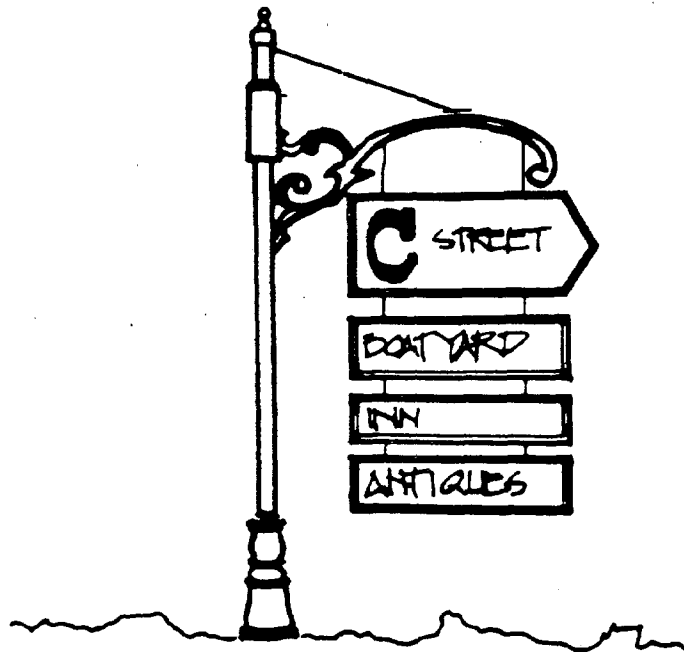


fig. 26

SOLOMONS MASTER PLAN

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Business Sign

CHAPTER IV

WATERFRONT DEVELOPMENT GUIDELINES

CHAPTER IV

WATERFRONT DEVELOPMENT GUIDELINES

A. INTRODUCTION: Waterfront Development Considerations

1. 100 Year Flood Plain
2. Shoreline Erosion
3. Highly Erodable Soils
4. Water Depth and Boat Traffic
5. Off-Site Impacts from Waterfront Development
6. Size of Land Parcels
7. Visual Impacts
8. Water Quality
9. Wetlands
10. Steep Slopes

B. Waterfront Development Guidelines

1. Waterfront Setbacks
2. Shoreline Protection Measures along the Patuxent River and Back Creek.

C. Waterfront Development Impacts

1. Expansion of Bulkheading into Patuxent

2. Proposed Bulkheading on Back Creek

D. Waterfront Development Permits

1. Permits Required for Waterfront Development

A. INTRODUCTION: Waterfront Development Considerations.

The land area contained within the Solomons Town Center includes some large parcels of undeveloped land best characterized as rural waterfront. In contrast, the Town Center also includes some of the most highly developed waterfront in Calvert County within the historic Solomons commercial area.

Current real estate market preferences clearly favor waterfront property for financial investment as well as quality of living reasons. However, this preference for living on the waterfront brings with it a large number of potential problems regarding impacts to the environment, and in some cases, possible hazards to the waterfront residents. Many of these environmentally destructive or potentially hazardous situations are the result of development occurring too close to the highly dynamic natural environment of the shoreline.

In the process of developing the waterfront, many considerations must be weighed if the construction is to be in harmony with the natural environment. The following paragraphs look at the development siting problems that have the greatest impact in the Solomons Town Center area. These waterfront development considerations have been incorporated into the land use recommendations.

It is important to understand that making generalizations about large sections of land, especially waterfront property, is done as a practical matter strictly for planning purposes.

Within any planning area or subarea, there will be variations in the natural conditions. Thus, a full site plan review evaluation will be necessary before development permits are granted.

1. 100 Year Flood Plain.

Coastal flooding occurs naturally as severe ocean-based storm systems (hurricanes, tropical storms and extra-tropical storms), create higher than normal tides, called a storm surge.

The amount of risk any particular shoreline has from such a storm is determined by the degree of exposure to larger than normal waves driven by high winds, and the relative elevation of the waterfront property above the storm surge and high waves.

The 100 Year Flood Plain characteristics for the shoreline of the Solomons Town Center is found in the following illustrations. (See Figures 11,15,17,18,20,22).

2. Shoreline Erosion.

Shoreline erosion characteristics fall into two broad categories: areas of low wave energy shoreline; and areas of high wave energy shoreline. The responses to each differ.

For low wave energy areas, a general rule of thumb is to avoid structural shore erosion protection such as bulk-heading and stone revetment when there is no serious erosion problem. In many cases a non-structural approach is not only preferable environmentally, but just as effective and much less expensive.

The practice of regrading the shoreline and planting marsh grasses to stabilize the shore has been used in many areas of the Chesapeake Bay for the last 15 years. This technique has not only proven successful in greatly reducing shoreline erosion in low wave energy areas, but it has the added benefit

of creating valuable natural habitat areas for many important estuarine species as well as functioning as a sediment trap which reduces sediment runoff from adjacent upland areas.

In high wave energy areas where there is a rapid retreat of the shoreline, structural approaches are often the only methods that will offer substantial, long-term stabilization. However, care must be taken when contracting for such work so that the design of the revetment or bulkhead will withstand the normally anticipated high winds and storms.

3. Highly Erodable Soils.

Some of the shoreline in the Solomons Town Center is characterized as having highly erodable soils. These soils are difficult to stabilize, and runoff from rain storms will rapidly carry the top soil into the adjacent waterbody. This type of erosion (upland erosion) contributes heavily to the "silting in" of the navigable waterway, and is highly destructive environmentally.

Development of these soils should be avoided wherever possible. In addition, steps should be taken to stabilize these areas and any planned use of these areas should be reserved for activities which will not aggravate the erosion problem.

Areas of highly erodable soils in the Solomons Town Center are identified in Figures 11,15,17,18,20, and 22.

4. Water Depth and Boat Traffic.

The siting of boating facilities in the upper areas of rivers and creeks is not only destructive to the natural environment, but also creates navigational problems in constrained waters.

It is the policy of the State Department of Natural Resources to discourage such practices and it is the recommendation of this Master Plan to follow that policy.

Solomons Island and the adjacent water have many excellent shorelines that can provide new sites for recreational boating facilities as they are needed. There is no specific point within a creek or waterway for drawing a line to limit boating activity, however, if any new boating facilities are to be constructed in the Solomons Town Center waterways above those that now exist, such facilities should be primarily for the wet storage of boats and should not be full service marina operations.

5. Off-Site Impacts from Waterfront Development.

There are two primary types of off-site impacts from waterfront development:

- a) adjacent land use - directly adjacent or across the water, and
- b) navigation in the vicinity of the new development.

There are other obvious adverse impacts such as glare and odor, but the most serious potential impact is that of noise. Because of the ability of sound to travel great distances over water, off-site noises from waterfront activities will very often become a chronic nuisance to property owners directly across the water or even farther away.

This problem of noise, which can seriously reduce the desirability of waterfront living, is accentuated by the practice of clear cutting the waterfront around residential properties. Clearing the waterfront of all tall standing trees and scrubs allows on-site sounds to leave the property toward the water, and also destroys the buffering effect that trees have on off-site generated sound.

As a waterfront area develops, the ambient noise level steadily grows louder. An overall increase in the ambient noise level can't be avoided. However, the nuisance problem can be significantly reduced, if not altogether eliminated, by requiring adequate landscape buffering along the waterfront in new development and encouraging the planting of trees along previously developed shoreline.

A final consideration on off-site impacts is the effect of exterior lighting on nighttime navigation. Exterior lighting around walkways, docks or buildings can create confusion for the boater at night, if there are also lighted navigational aids in the area. If such exterior lighting is to be used on the waterfront, then light deflectors should be used to avoid this problem.

6. Size of Land Parcels.

The size of land parcels available for new development is a major consideration in waterfront development. Many activities associated with waterfront development, aside from single family detached residential, will require a substantial amount of acreage if the negative environmental consequences of development are to be minimized or avoided.

7. Visual Impacts.

Preservation of the visual qualities and character of the Solomons Town Center will enhance the quality of life for present residents and future generations. Such preservation will also substantially enhance the value of the area and increase its marketability.

Current preferences in residential development seem to favor the clear cutting of trees and scrubs and the construction of bulkheading, regardless of need. These practices need to stop in the Solomons area if the rural character along Upper Back Creek and St. Johns Creek that now exists is to be maintained.

8. Water Quality.

The primary water quality problems in the waters around the Solomons Town Center area are sedimentation from shoreline erosion and upland runoff, and organic pollutants

from failing septic systems along the waterfront.

There are many other sources of water pollution in this area that are associated with boating, commercial fishing and stormwater runoff from streets and parking areas that combine to create considerable impact as well. These stem from the active use of the waterfront. And it is clear, that the greater the activity along the waterfront, the greater the impact on water quality will be.

Previous development, for the most part, around Solomons has been along the waterfront near the mouth of St. Johns and Back Creek where the normal movement of the tides are constantly flushing pollutants out of the area.

Development which occurs near the headwaters of Back Creek and St. Johns Creek, however, will produce pollution, which if not carefully managed, will have little chance for flushing into the Bay. This will result in poorer water quality for greater part of the Town Center area. Consequently, the areas directly adjacent to the upper halves of these Creeks should be developed in a way which minimizes water quality problems. One way to achieve this is to cluster new development and reduce the amount of impervious surfaces which surrounds the development. This policy should be a high priority in building permit decisions in these Creek areas.

9. Wetlands.

The State of Maryland and the Federal government regulate development on or around tidal and non-tidal wetlands. Tidal wetlands are defined by the State of Maryland as any land under the navigable waters of the State below the mean high tide, affected by the regular rise and fall of the tide.

Tidal wetlands are recognized for their importance as a principal source of food for the animals that inhabit the waters of the Chesapeake Bay. These areas also provide significant pollution abatement as the incoming tide floods over the standing marsh grasses, by increasing the levels of dissolved oxygen and reducing the levels of nitrate nitrogen and phosphates in the water. In addition, these areas also

provide essential open spaces along the waterfront.

Recently, the State of Maryland has begun the regulation of development on non-tidal or upland wetlands. These areas are not as easily identified as tidal wetlands, but their importance in the recharging of ground water supplies and as sensitive natural habitats has been established.

New regulations, currently being developed by the State of Maryland, for land use associated with these areas will require substantial setbacks and protection from grading, filling and the effluent from stormwater runoff.

10. Steep Slopes.

The slopes around the Solomons Town Center area are for the most part not very high, however care must be taken to see that development does not occur too close to these areas. The unstable soils often associated with steep slopes should either be protected from contact by foot traffic or other more intense uses, or should be properly stabilized.

B. WATERFRONT DEVELOPMENT GUIDELINES.

These waterfront development guidelines have been developed, based on the waterfront considerations above, the critical areas initiative of the State of Maryland, and the stated goal of this Master Plan to address the issues of protection and enhancement of Solomons water resources heritage.

1. Waterfront Setbacks.

A construction setback of 100 feet is recommended for all waterfront areas within the Solomons Town Center. Specific exceptions from this requirement are detailed in Chapter III: Permitted Land Uses and Requirements.

This construction setback of 100 feet would provide sufficient area for institution of non-structural stormwater management measures such as the maintenance or creation of vegetative buffer areas, and the institution of vegetative shoreline stabilization measures. Limited active use may occur within this 100 foot setback, but no impervious surfaces will be allowed.

The establishment of such a setback and maintenance of vegetative buffering is consistent with the requirements of the newly enacted Chesapeake Bay Critical Areas Act. This Act requires local jurisdictions to develop management programs for a 1000-foot strip landward of tidal waters or wetlands which minimize adverse impacts on water quality from point and non-point discharges and conserve fish, wildlife and plant habitat. They are also required to address the secondary impacts from development

In addition, this construction setback permits an area for the effective management of stormwater treatment as well as maintaining a vegetative buffer area along the shore. Such a setback also ensure that no construction will occur within the 100-year floodplain, and thus reduces the possibility of damage from flooding.

The following illustration of Shoreline Treatments for various types of construction provide visual guidance for environmentally effective management of the shore.

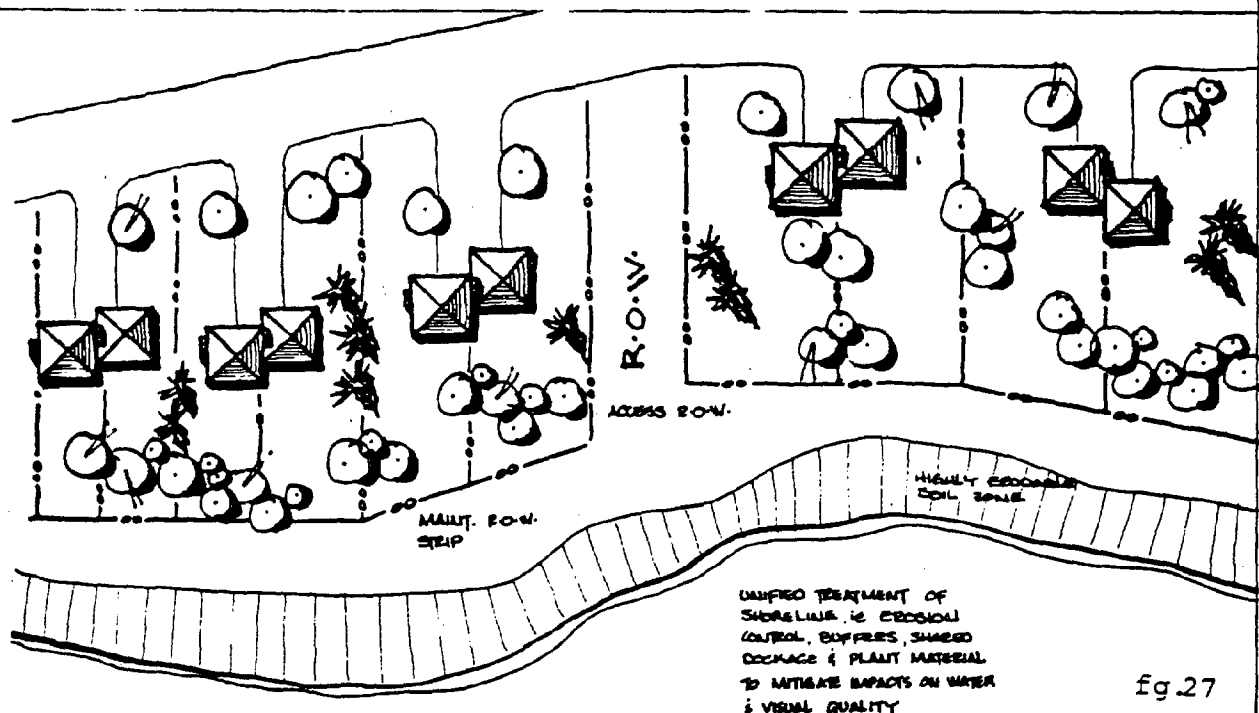
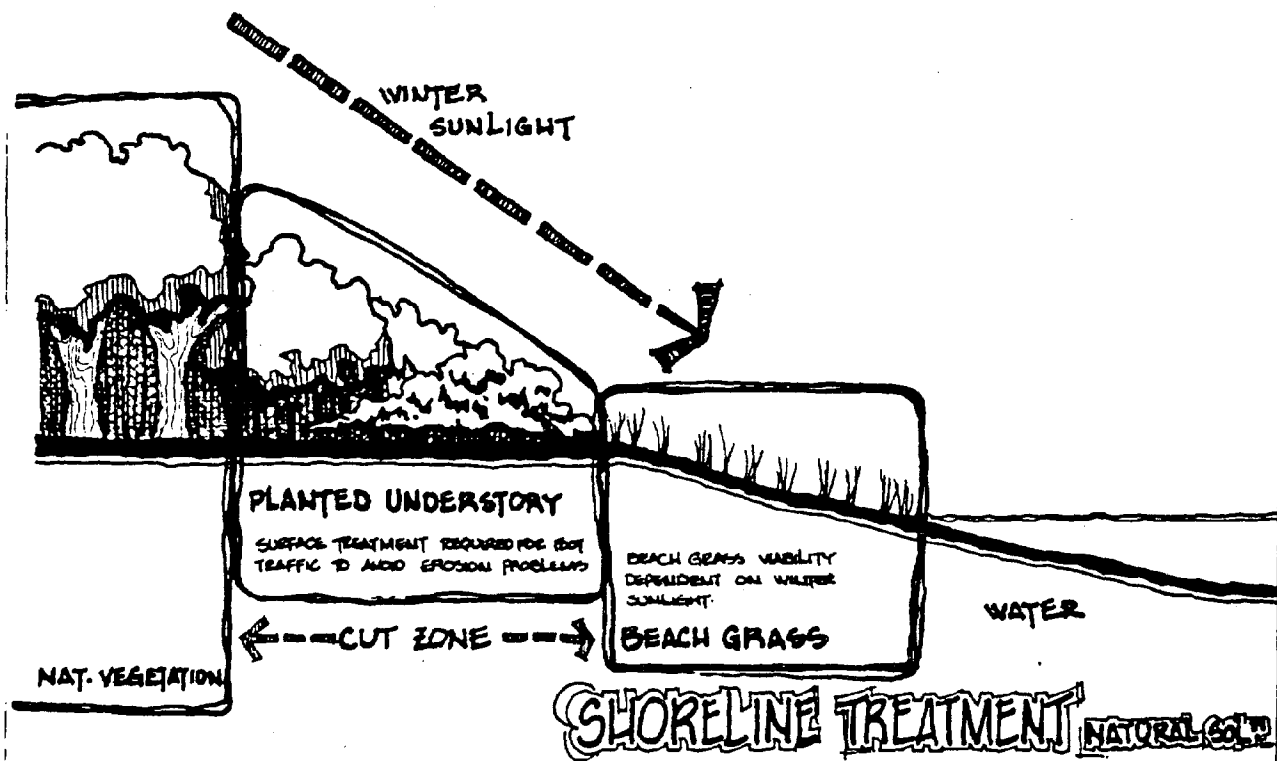


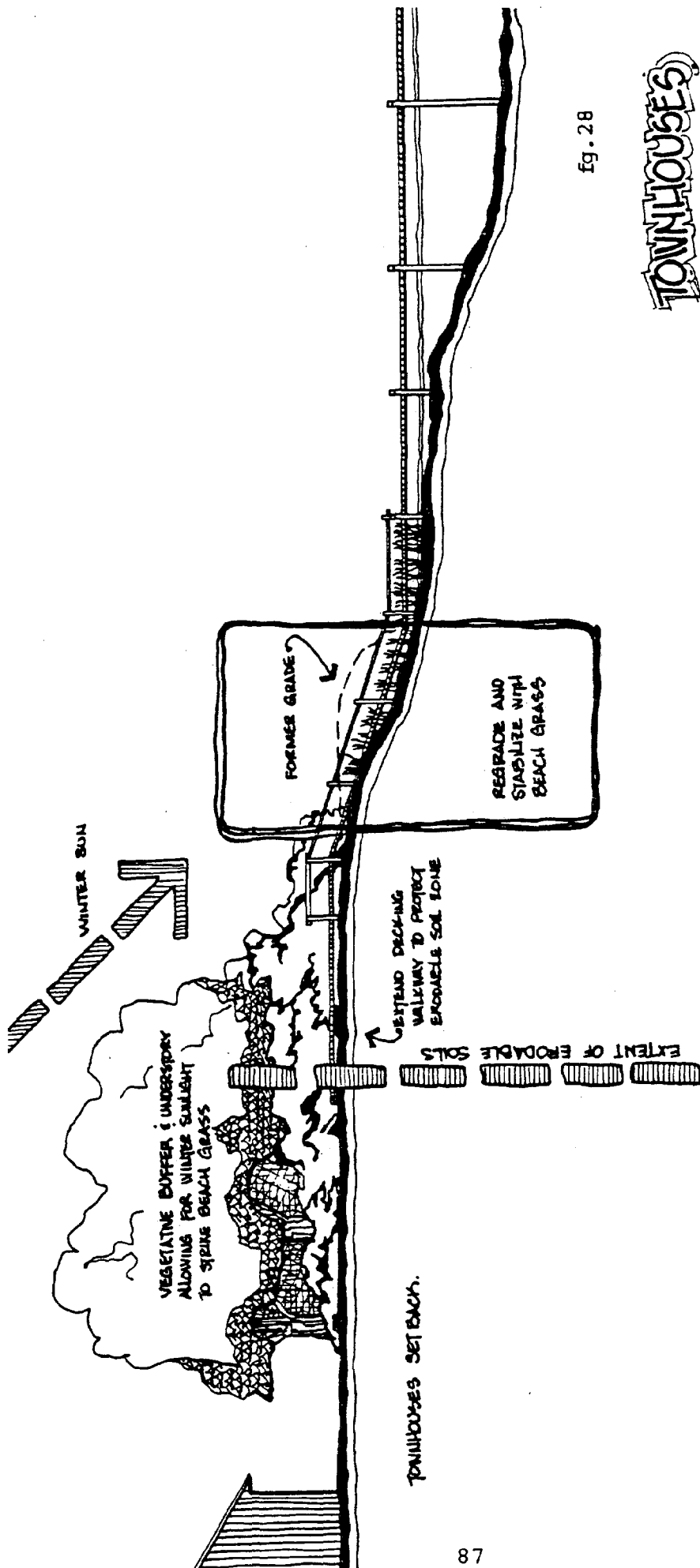
fig. 27

MULTIPLEX

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Shoreline Treatment
Multiplex



SOLOMONS MASTER PLAN

PREPARED FOR CALVERT COUNTY BY THE PIPPS GROUP, J.M. HUTTO ASSOCIATES AND ALAN SPARBER & ASSOCIATES

Shoreline Treatment Townhouses

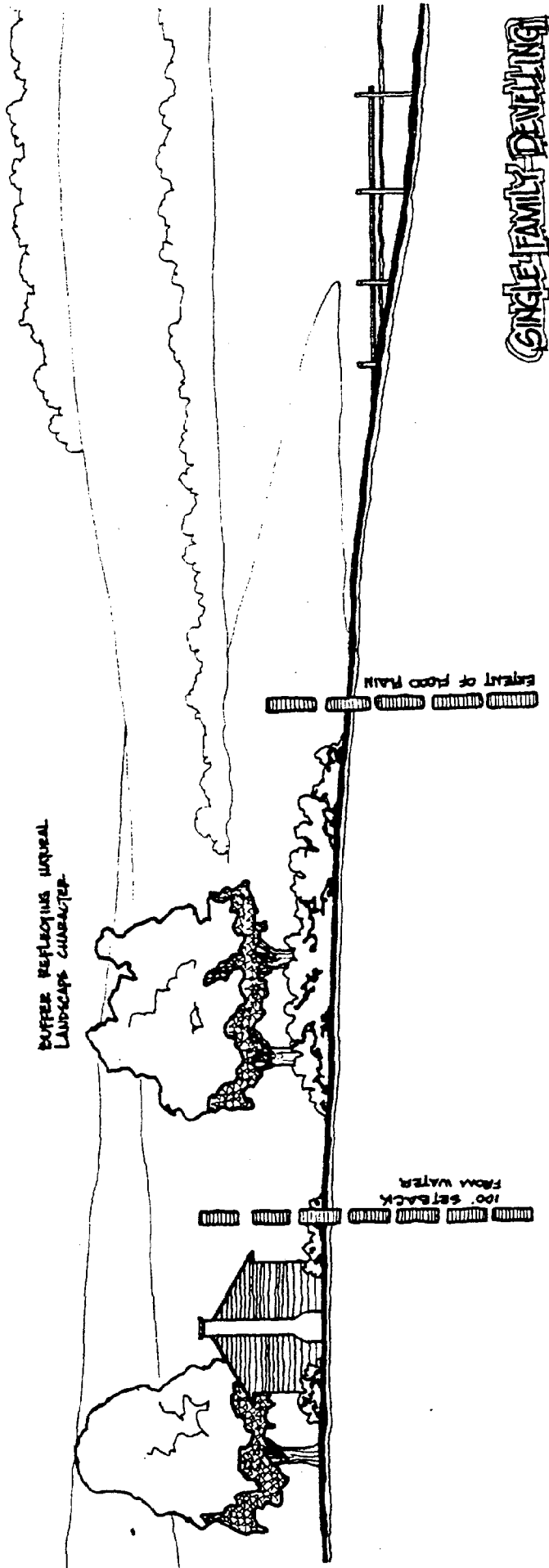


Fig. 29

SOLOMONS MASTER PLAN

Shoreline Treatment
Single Family

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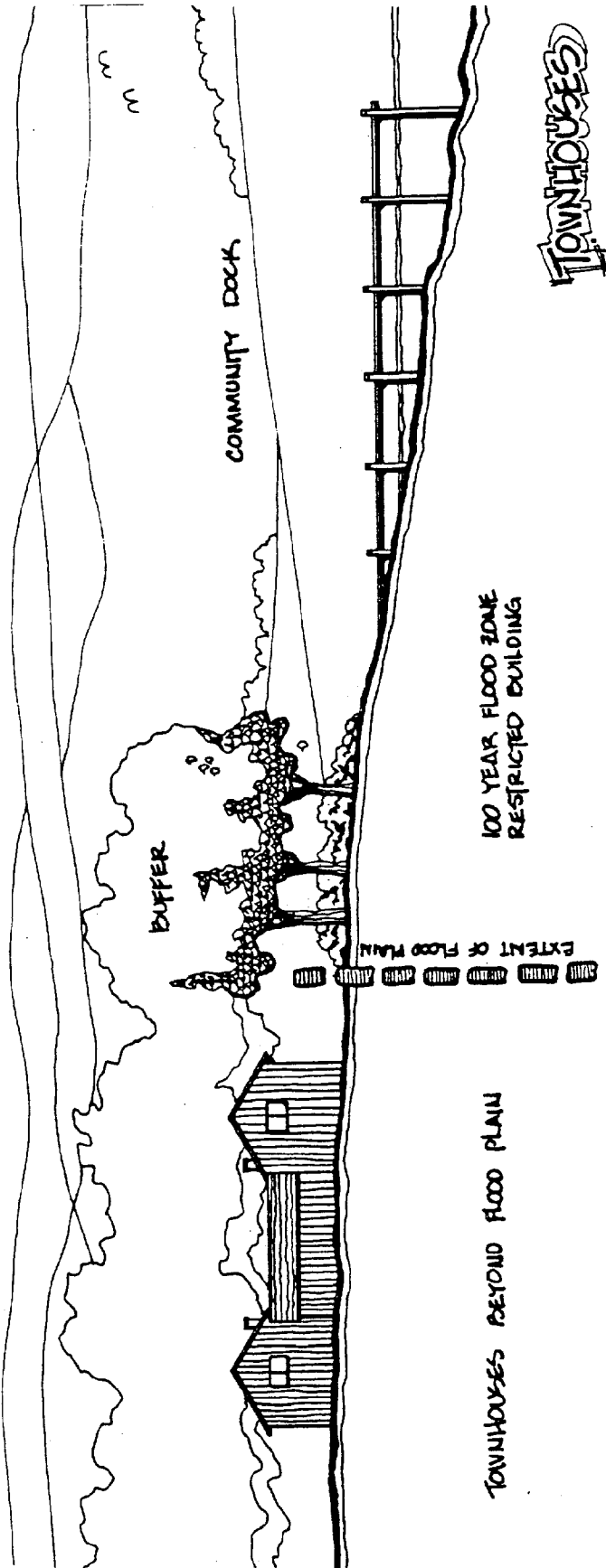


fig. 30

SOLOMONS MASTER PLAN

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**Shoreline Treatment
Community Dock**

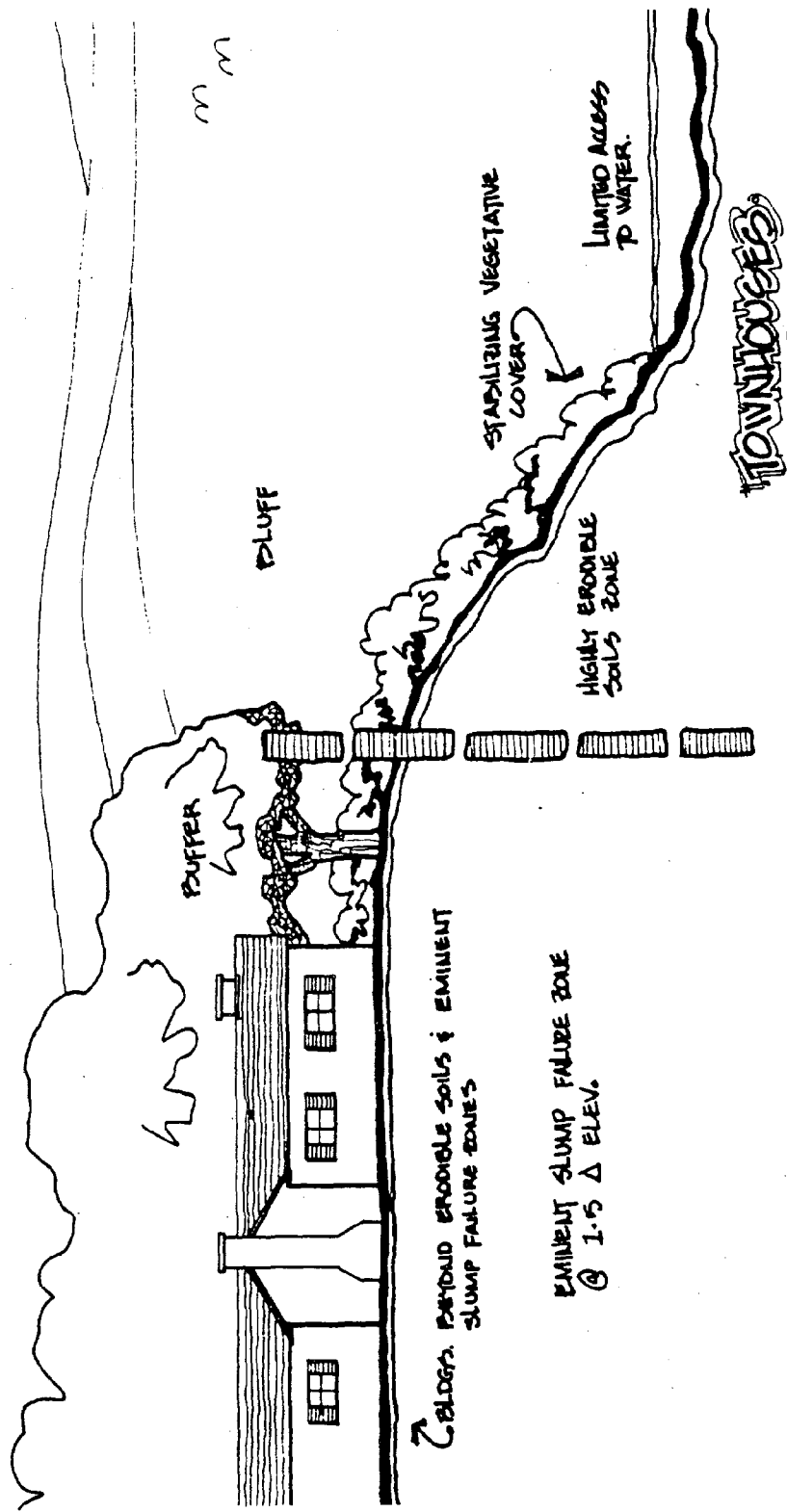


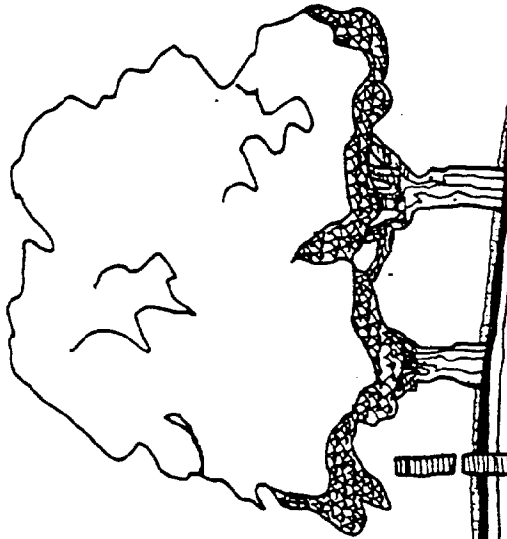
fig. 31

SOLOMONS MASTER PLAN

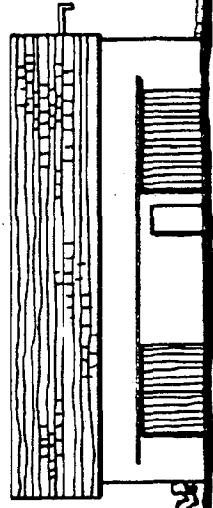
Shoreline Treatment
Highly Erodible Soils

PREPARED FOR CALVERT COUNTY BY THE Phipps Group, J.M. Hutto Associates and Alan Sparker & Associates

BUFFER.



PARKING AREA TO DRAIN AWAY FROM SHORELINE



BUILDING AREA

EXTENT OF ERODIBLE SOIL

- HIGHLY ERODIBLE SOIL ZONE
- PERMANENT SURFACE TREATMENT (PATH) BEYOND THIS ZONE TIES TO DOCK AREA TO BULKHEAD

100' SETBACK FROM SHORELINE

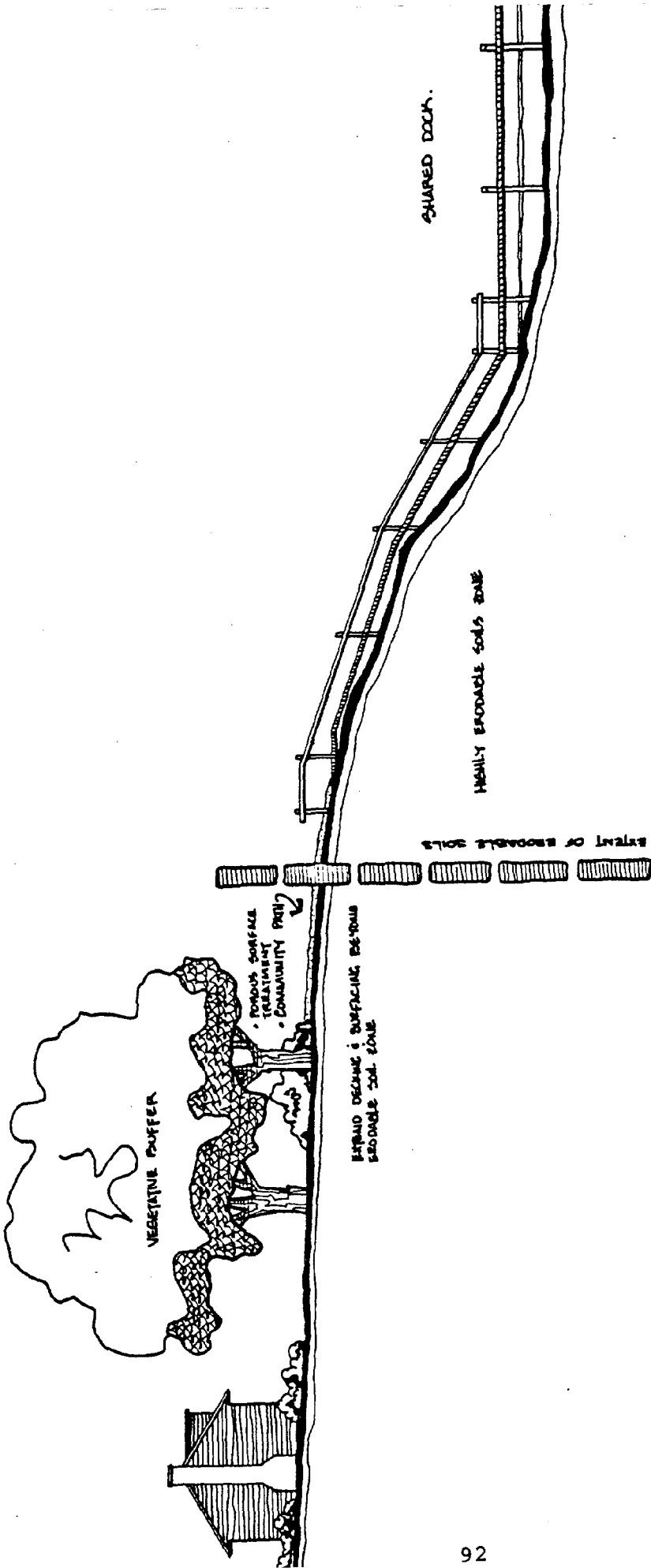
MARINA

Fig. 32

SOLOMONS MASTER PLAN

Shoreline Treatment
Marina

PREPARED FOR CALVERT COUNTY BY THE Phipps GROUP, J.M. PHIPPS ASSOCIATES AND ALAN SPARBER & ASSOCIATES



SINGLE FAMILY DWELLING

fig. 33

SOLOMONS MASTER PLAN

Shoreline Treatment
Highly Erodible Soils

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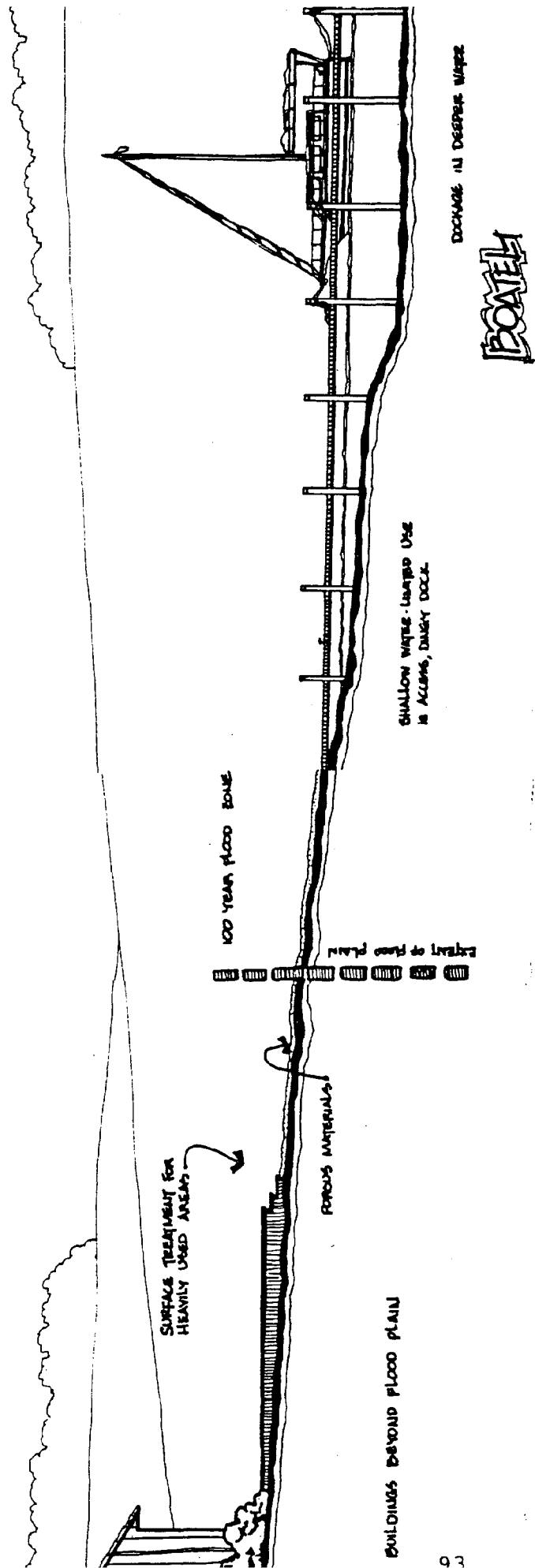


Fig. 34

SOLOMONS MASTER PLAN

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Shoreline Treatment
Boatel

2. Shoreline Protection Measures along the Patuxent River and Back Creek.

a. Vegetative Stabilization.

Vegetative stabilization is the recommended method for shoreline stabilization in areas without high wave and wind action, and where a vertical face is not needed to allow boats to come directly alongside.

b. Sloping Stone Revetment.

If structural measures are needed, a sloping stone revetment (rip-rap) is the preferred method of shoreline protection. Its advantages are: the materials do not degrade with time; it is not likely to fail catastrophically during a storm; stones dislodged can be recovered and replaced; it provides a better habitat for biota than a vertical structure; wave run-up and overtopping is less; and wave reflection is lower, resulting in less disturbance and scour at the toe.

In contrast, a vertical wooden bulkhead is more likely to fail due to scouring at its bottom or eroding of the area behind it. Moreover, its vertical face will reflect wave energy which may cause increased scour in front of the bulkhead. It protects only the area immediately behind it, and offers no protection to adjacent areas up and down the shore, or to the areas in front which may still experience erosion. Additionally, bulkheading needs to be adequately anchored; have adequate toe protection from scouring; and be high enough to prevent overtopping. Its advantage is the vertical surface it offers to boat tie-up.

The following illustrative Site Sketches provide guidance for siting various types of development in relationship to the shore.

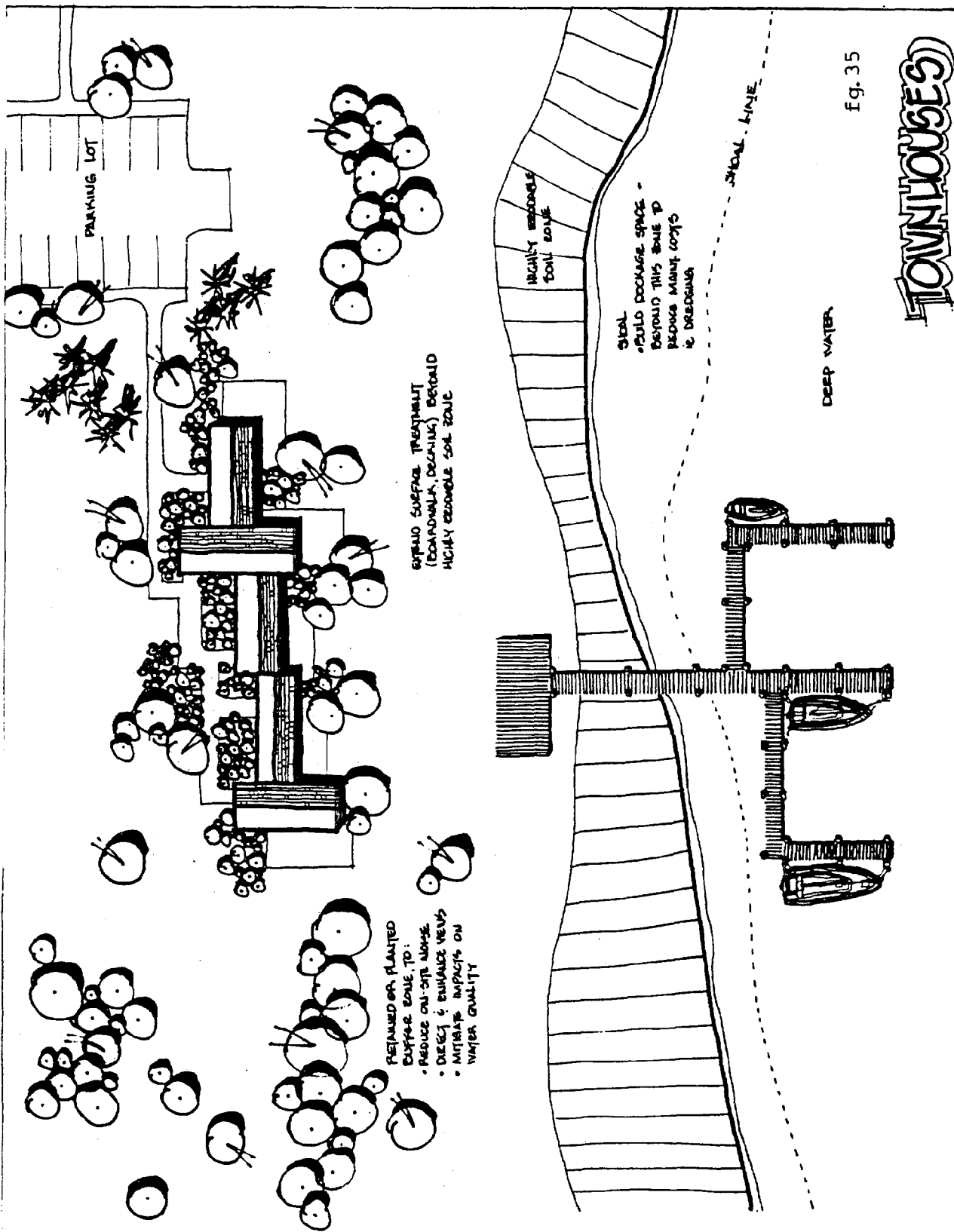


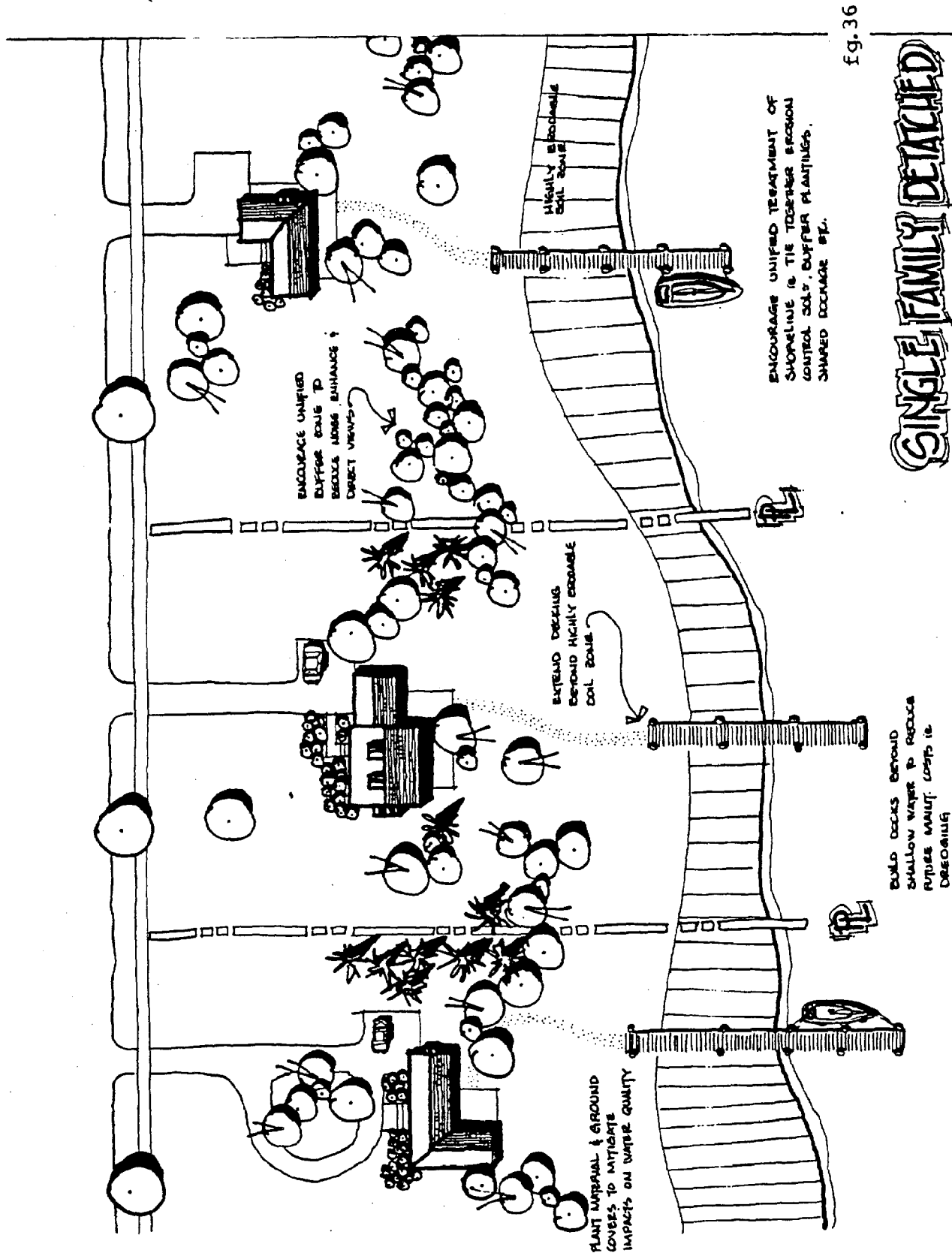
Fig. 35

TOWNHOUSES

SOLOMONS MASTER PLAN

PREPARED FOR CALVERT COUNTY BY THE PHIPPS GROUP, J.M. HUTTO ASSOCIATES AND ALAN SPARBER & ASSOCIATES

Site Sketch
Townhouses



SOLOMONS MASTER PLAN

PREPARED FOR CALVERT COUNTY BY THE PHIPPS GROUP, J.M. HUITO ASSOCIATES AND ALAN SPARBER & ASSOCIATES

Site Sketch
Single Family

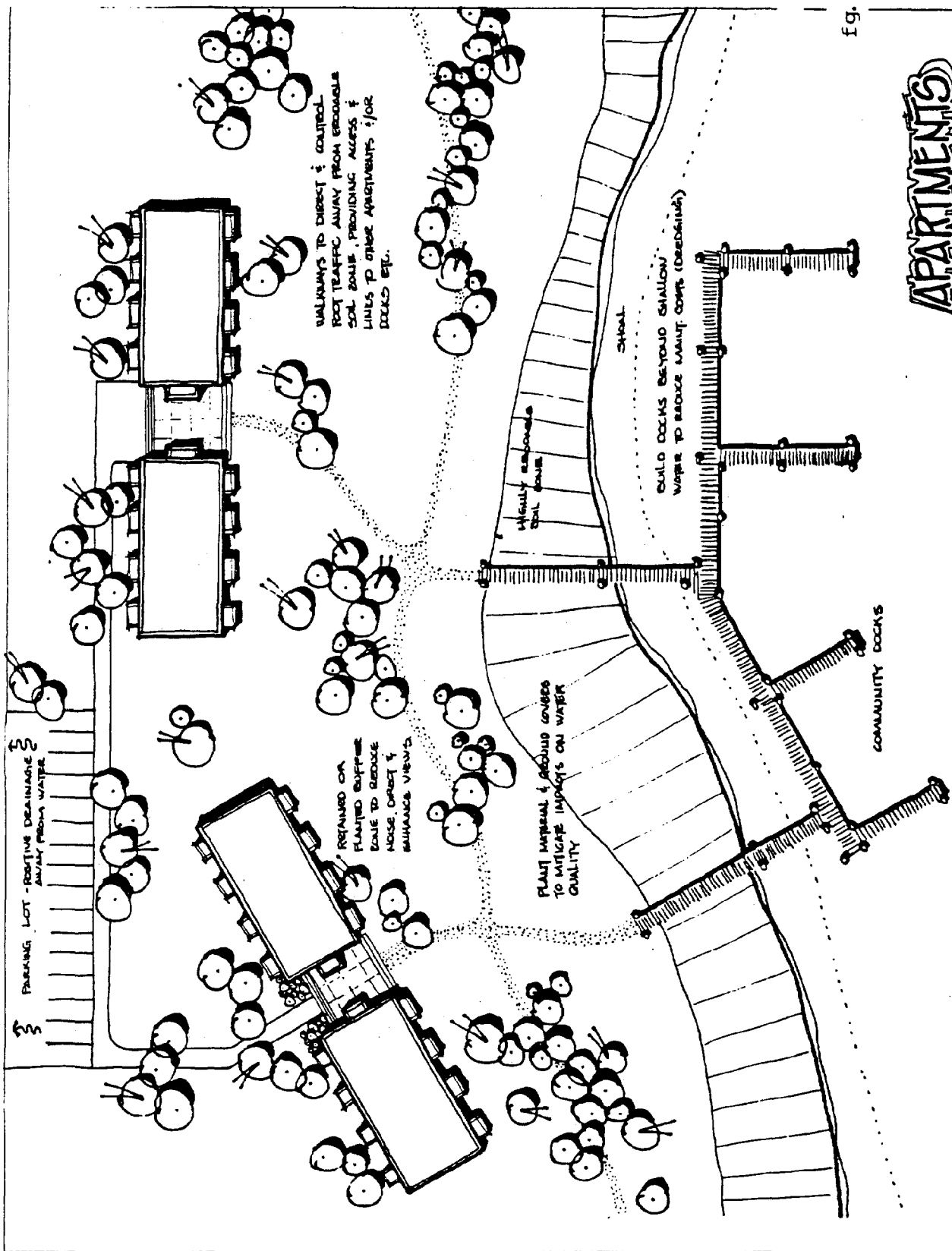


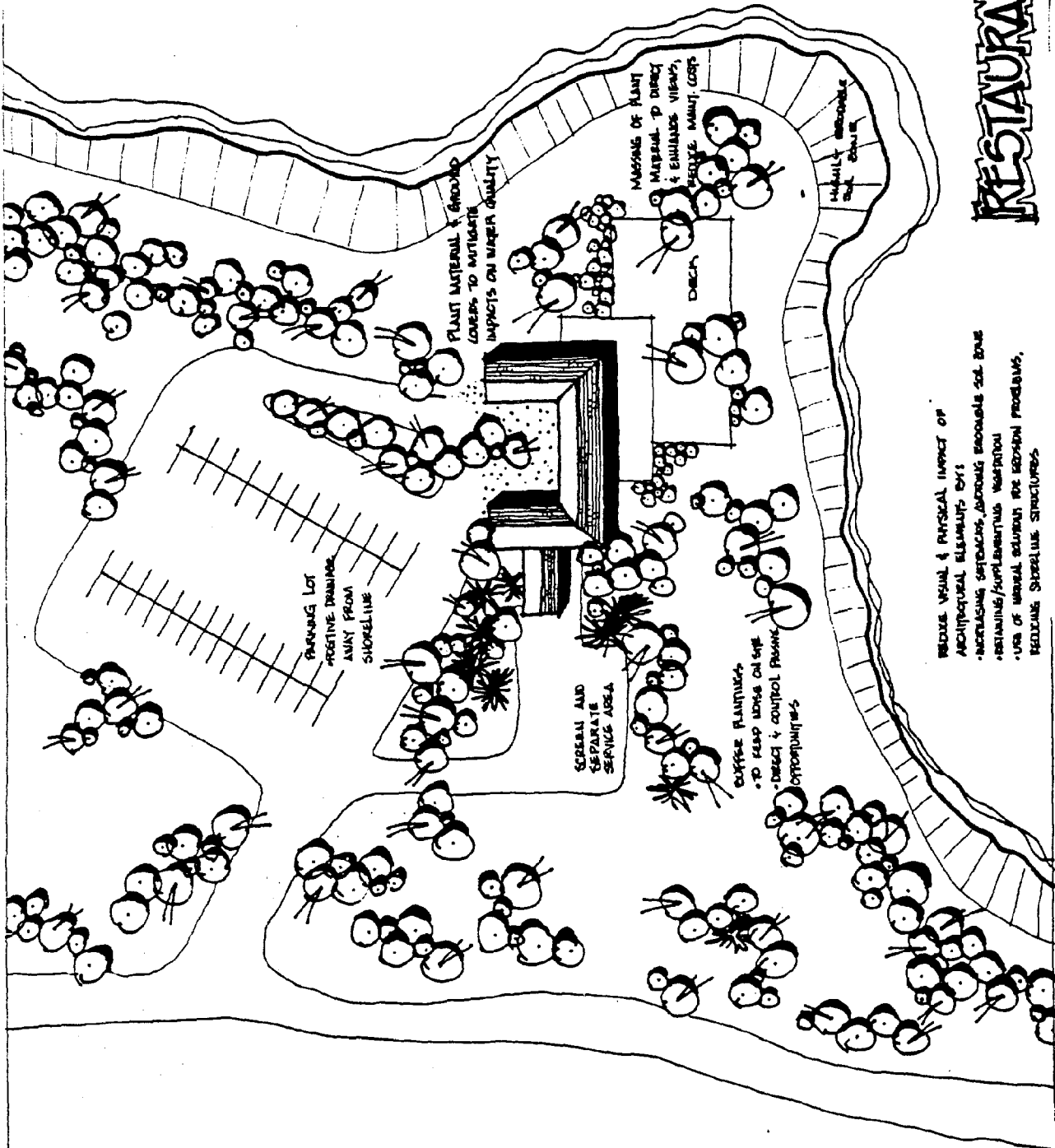
fig. 37

APARTMENTS

SOLOMONS MASTER PLAN

PREPARED FOR CALVERT COUNTY BY THE PHIPPS GROUP, J.M. HUTTO ASSOCIATES AND ALAN SPARBER & ASSOCIATES

Site Sketch
Apartments

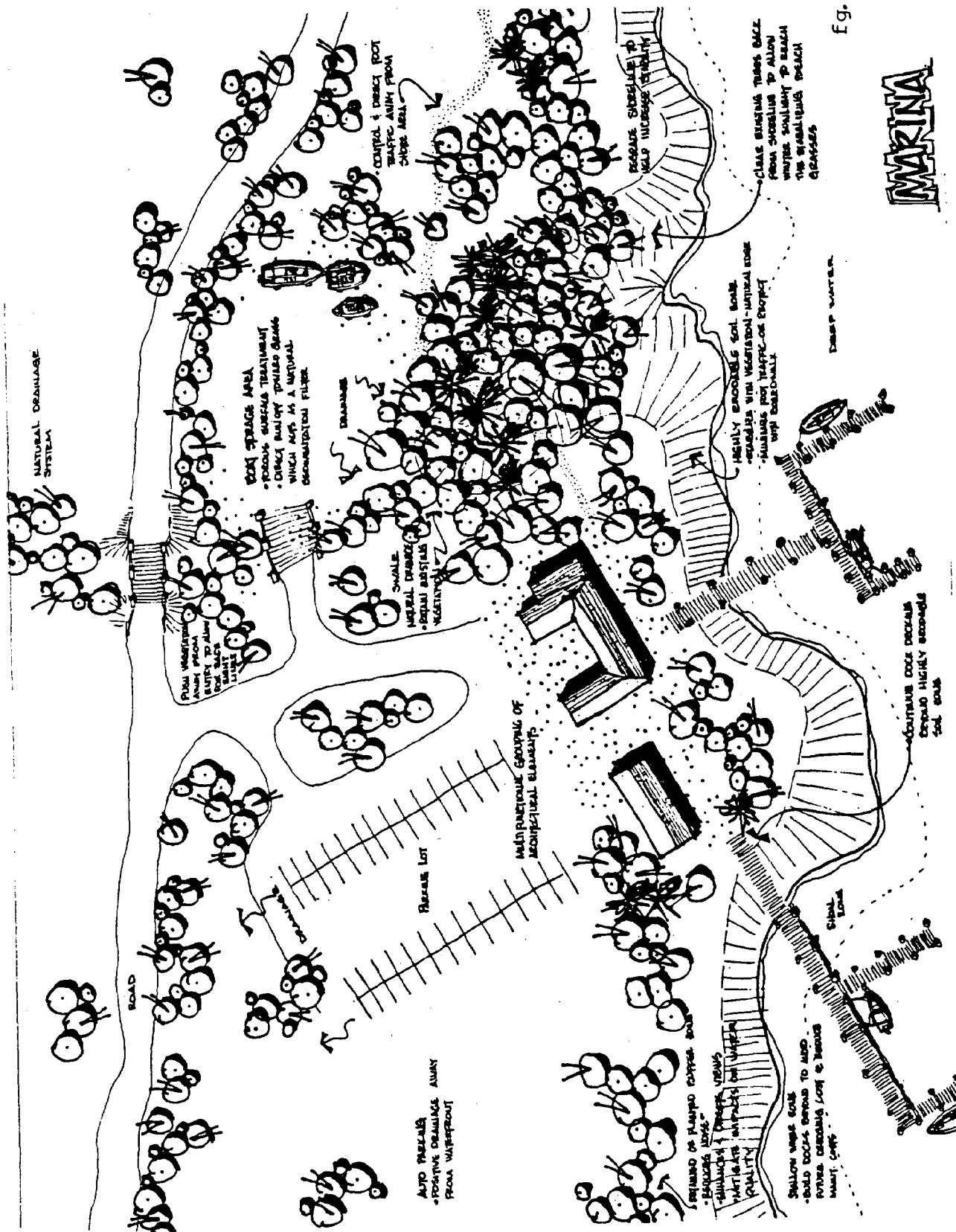


RESTAURANT

SOLOMONS MASTER PLAN

Site sketch
Restaurant

PREPARED FOR CALVERT COUNTY BY THE PHIPPS GROUP, J.M. HUTTO ASSOCIATES AND ALAN SPARBER & ASSOCIATES



SOLOMONS MASTER PLAN

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Site Sketch Marina

C. WATERFRONT DEVELOPMENT IMPACTS.

A principal recommendation of this Master Plan is the expansion and additional construction of bulkheading along the Patuxent and Back Creek. (See Chapter V: Public Improvements for details).

1. Impacts of Bulkhead Construction along the Patuxent.

The construction of bulkheading along the Patuxent River and in Back Creek will create impacts on adjacent estuarine natural systems as well as the adjacent property owners. Impacts on the natural systems can be categorized within four major areas:

- o water quality,
- o erosion/sedimentation/deposition,
- o flood heights, and
- o ecology.

Water quality impacts associated with the siting and construction of bulkheading focus primarily on the increased water turbidity from the resuspension of bottom sediments into the water column. However, this impact is temporary and normally not associated with significant levels of environmental degradation.

Impacts relating to erosion/sedimentation/deposition may affect the circulation patterns of water with an ultimate transfer of destructive energy of waves downstream or upstream of the bulkheaded shoreline. Such impacts often involve scouring at the toe of the bulkhead and unprotected adjacent shorelines

may erode because of the undissipated wave energy (reflective wave energy). However, because the shorelines considered here are currently bulkheaded or stabilized in some way, these impacts would have been evident by now. It can be anticipated that a new equilibrium will be reached by the water body shortly after termination of construction.

Any new construction of bulkheading in the historic Solomons center will have little effect on flood heights because of its proximity to the mouth of the Patuxent River and the Chesapeake Bay.

The impact of construction of bulkheading in this area on the estuarine ecology will be primarily to the benthic communities (those living on the bottom) in the immediate area of the proposed construction. Some of the benthic habitat will be temporarily impacted, while filling of the area behind the bulkhead will destroy organisms living there.

The mud/sand shoal adjacent to the entire eastern side of Solomons Island, including the area adjacent to the public parking lot, is a very large area approximately 1700 yards long and 150 yards wide. This entire shoal area has similar depths throughout its length of between one and three feet. The loss of habitat through the extension of the bulkhead into the Patuxent River another 70 feet should not produce any significant threats to existing benthic communities. The extension of the bulkheading should not impact any marsh communities, feeding, breeding or nursery area for fish or shellfish. The invertebrate communities in this area will probably be the organisms most affected by this construction.

2. Height of Bulkhead along the Patuxent.

Water depths adjacent to the public parking lot are between one and three feet at mean low tide. Wind conditions vary seasonally, but due to the large exposure and sizeable fetch to the South, East, and Northeast, this shoreline offers no opportunities for long-term mooring of boats. However, the bulkhead can be used for temporary on and off loading during periods of still air.

If the bulkhead along the parking lot is to be used by boats, the shoaling problem will have to be addressed for boats that draw more than a few feet of water.

The storm surge heights for this area are estimated at approximately seven feet. The existing roadway behind the bulkhead is at an elevation of between five and eight feet, while the bulkhead is a foot or two lower than the roadway. It is desirable to have the bulkheading increased in height to avoid overtopping to the greatest degree possible.

3. Proposed Bulkheading on Back Creek.

Water depths in this area are highly suitable for boat usage. This is approximately 13 feet of depth adjacent to the shoreline.

This areas is naturally protected by the adjacent shoreline configuration with no sizeable open water fetch to produce destructive waves. The storm surge heights for this area are estimated to be at approximately seven feet. The area directly adjacent to the site of the proposed bulkheading is between three and four feet, however, normal storm winds don't appear to be creating much overtopping of existing structures with equally low heights. This is probably due to the protect nature of this shoreline.

D. WATERFRONT DEVELOPMENT PERMITS.

1. Permits Required for Waterfront Development.

The construction of marine facilities including bulkheads and piers must comply with Federal, State, and local regulations.

Calvert County Zoning Regulations establish harbor lines beyond which piers cannot extend; prohibit any marine

facility in the usable waterway if the width of the waterway is 60 feet or less; and provide for the establishment of lateral lines within which piers must be constructed. These regulations also require a county building permit for the construction of any marine facility and a grading permit for any dredging, filling or other activity which modifies the shoreline or the bottom of a waterway.

Dredging, filling, and the construction of shoreline protection devices also require a Corps of Engineers Section 10 and/or 404 permit, as well as the Associated State Coastal Zone Management Consistency determination and Water Quality Certification. E.P.A., the U.S. Fish & Wildlife Service, and the National Marine Fishery Service provide advisory comments to the Corps of Engineers for it to consider in making its permit decisions.

The objectives of the Corps' permit system is to ensure that the adverse environmental and water quality impacts of the proposed projects are minimized, and that navigation is not obstructed. A State wetlands license and/or permits is also required. The purpose of the State permit/license system is to ensure that impacts on vegetated wetlands and tidal waters are minimized.

Applications for piers of reasonable length and for shoreline protection measures adjacent to shoreline are usually approved by Federal and State agencies. Significant expansion out from the shore, such as the one proposed for the Patuxent bulkhead, must demonstrate that they are for the common good, and part of an entire shoreline protection plan.

CHAPTER V

APPEARANCE GUIDELINES

CHAPTER V

APPEARANCE GUIDELINES

A. Introduction

1. What are Appearance Guidelines?
2. Role of Appearance Guidelines.

B. Historic and Architectural Perspective

1. Historic Buildings and Sites.
2. Community Participation and Town Center Ordinance.

C. Appearance Guidelines

1. Architectural Design Features.
 - a. Building Materials
 - b. Facade and Roof Colors
 - c. Window Detail
 - d. Doors and Entryways
 - e. Ornamentation
 - f. Rooflines and Pitch.
2. Treatment of Accessories
 - a. Fences
 - b. Signage
 - c. Lighting

A. INTRODUCTION

The present attraction to Solomons has more to do with location and geography than with the appearance of the public spaces and private buildings which make up the physical structure of the town. It is actually a tribute to the strength of that attraction to the water that brings residents and visitors to the Island, in spite of the rather bleak appearance of much of the waterfront.



ENTRANCE TO SOLOMONS ISLAND
1984

fig. 41

However, the Island has not always had this same appearance. Since the early 18th Century, Solomons was a vibrant, working waterfront community, dependent on the Chesapeake Bay for its livelihood, and with an architectural style befitting the time and the functions of its buildings. Compare the

photo above with the one below which reflects the approximately the same waterfront area in 1909.¹



SOLOMONS ISLAND, NORTH OF THE TIDE BOX
(From postcard dated 1909)

fig. 42

1. What are Appearance Guidelines?

Appearance guidelines are that set of principals which reflects the original design of Solomon's traditional residential and commercial buildings. The guidelines thus incorporate those visual and architectural characteristics common to the older styles and builds on them using those characteristics as a foundation for future work.

¹R. Eschleman, Historical Tours through Southern Maryland, p.9.

2. Role of Appearance Guidelines.

The role of appearance guidelines is not to reproduce any particular architectural style, and overlay some preconceived appearance on each and every structure in the area. Their role is to protect, as new buildings are built, the continuity and architectural heritage of Solomons in a way which reinforces the existing fabric of the town.

Within that heritage today are a wide variety of styles and settings, and, as a result, many different kinds of new buildings can be compatible; however, some may not. For that reason, the intent of the guidelines is to be selective - and often restrictive - in order to exclude buildings that would erode the historic and cultural values built up in Solomons over generations. Thus, while not trying to imitate or replicate a style no longer practical, the overall design theme of the town, as documented in these guidelines, is consistent with the recorded older character.

The guidelines are intended to apply to new structures and exterior alterations; they are not written for historic restorations. These guidelines have been designed to be expandable. They can be made more comprehensive and more specific as the Appearance Review Committee requires. Moreover, strict adherence to these guidelines does not necessarily assure approval. Evaluation of buildings still requires the judgement of the Review Committee members; guidelines provide a framework for studying the plans submitted to them. The Committee still has to consider the full impact of each design as a whole, as well as its impact upon its neighbors.

B. HISTORIC AND ARCHITECTURAL PERSPECTIVE.

The historic values of Solomons are found in the design of individual buildings, the street scene, and the overall relationships of buildings, streets, topography, and water. The architectural and the street scene are a reflection of a society, economy, and technology very different from today's.

The overall pattern of streets, lots, and landmarks was set in the 18th and 19th centuries, and determined, to a large extent, by the nature of the oystering and shipbuilding industries which characterized its early years. As a result, the buildings, lot sizes and streets have what is called a "human scale." Street widths and building heights were designed for use by people arriving on foot or by boat.

Two key ingredients of this "human scale" are found in (a) the continuity of the street scene, and (b) height and scale. These characteristics are found in:

- heights and bulk - the overall size of buildings and their combined skyline,
- massing and building modules - the combination of building blocks or individual forms that are assembled into a total structure,
- roof forms,
- setbacks and yards,
- construction materials, colors, and textures,
- rhythms, proportions, and size of doors and windows - the patterns of repetition; the relation of widths to heights; the placement and spacing,
- construction details - cornices; window trim; decoration.

1. Historic Buildings and Sites.

Judging from the earliest remaining photographs and engravings of Solomons, the largest structures on the Island were utilitarian and of a marine industrial nature. The oyster cannery and boat buildings and repair sheds and other miscellaneous machine shops and factories were constructed as industrial buildings have always been built - as inexpensively as possible.

While many of the old building sites on the Island have local significance in terms of previous uses, none of the historical buildings that remain is of such architectural significance that it requires exact restoration to its original appearance. This is true even of the "Old Solomons House" and the Lore Oyster House, both of which have undergone extensive remodeling.

The most significant structure in the Solomons area, the Drum Point Lighthouse, is no longer on its original site. Moved to its present location on the grounds of the Calvert Marine Museum in 1975, the structure has been restored to its 1900 appearance, and constitutes a landmark for Southern Maryland. For this reason, it is vitally important that any new construction planned for this site should insure that the view of this landmark remain totally unimpaired.

In spite of the many losses to the built environment, Solomons, none-the-less, still conveys today some of the distinctive architectural features of a 19th Century Chesapeake Bay fishing community. Characteristics such as high peaked roofs and narrow wooden clapboard siding, attest to earlier 19th century construction. Later 19th century architecture is also evident in several fine examples of Victorian residences found along Solomons Island Road.

It is strongly recommended that the Historic District Commission develop a current inventory of those structures within the Solomons Island Town Center which possess either historical or architectural merit, and review the possibility of designating the area an historic district.

2. Community Participation & Town Center Ordinance.

During the public meetings held by the Calvert County Planning Department as preparation for this Master Plan, one of the needs most frequently expressed by the residents and business people of Solomons was the desire for architectural standards for buildings and signage.

In addition, the Comprehensive Plan process established

procedures for instituting Appearance Review within Town Centers wishing to institute such a code. These procedures call for the appointment of an Appearance Code Committee whose functions shall be to:

- o approve exterior design features of buildings, structures other than buildings, and signs on the basis of standards and guidelines contained within the appearance code.
- o promote awareness of good design within the community.
- o advise the County on matters relating to the design of public amenities within Town Centers and any other matters relating to appearance or design with Town Centers, and
- o review Master Plans and make recommendations.



C. APPEARANCE GUIDELINES

1. Architectural Design Features.

To develop compatible appearance guidelines, it is important to regard the entire street and the adjacent neighborhood as an ensemble which together creates the character of a place. By concentrating on the various rhythms formed by the window and door openings, by porches, by the relationship of solids and voids as you view a series of buildings, a pattern is established which should repeat in both renovation and new construction.

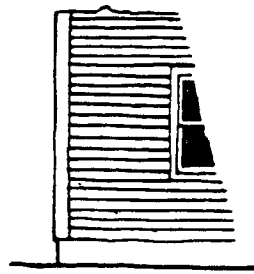
The existing pattern which distinguishes Solomons is characterized by a series of small window and door openings, high pitched roofs, frequent large dormers with a variety of pitches and open porches with a shed or gable roof.

a. Building Materials.

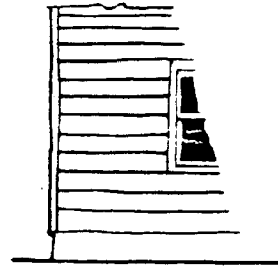
Clapboard siding is the predominant exterior facing on Solomons, and its proper use and repair should be encouraged. It was the most common facing of the wood frame buildings in this part of the country and is still being used with success in new construction today. It is the material of choice, wherever possible, for new construction in Solomons.

In clapboard siding, the horizontal wood strips overlap to keep out the weather, and in doing so cast continuous shadow lines across the facade. This characteristic shadow line is unique to the material, and is a feature of wood clapboards which has not been duplicated by the synthetic materials which are often used to replace it.

When wood siding is used, the spacing of the clapboards should be as close as possible to the original, as illustrated in the following drawings:



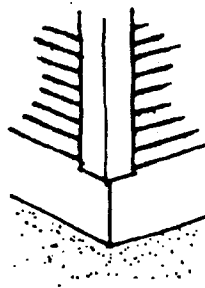
YES



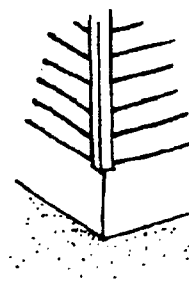
NO

Note how the apparent scale of the wall has changed because of the wider use of siding overlap, which also affects the texture of the wall surface.

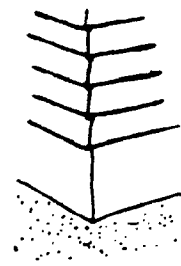
The corner treatment of siding is important because a wide corner strip was typical in the original construction. The later feature of mitering the corners, or using a small corner board do not have the same affect and do not frame the wall properly. This is shown in the following drawings.



YES



NO



NO

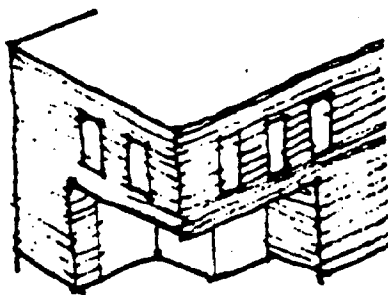
fg. 43

For very large new structures, it may be necessary to use brick. Where brick is used, the rhythm of windows and openings is equally important.

This technique was disregarded in the new buildings for the Biological Laboratory of the University of Maryland located in the Sandy Point area of Solomons. The new buildings, although attractive contemporary individual structures, are not related to the surrounding architectural or historical context in any significant way. Additionally, the large empty expanses of brick and large glass areas do not relate well to the residentially scaled buildings around them.

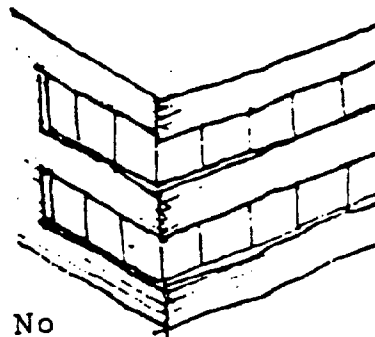
While the University's neo-Georgian revival Administration building built in the early 20th Century also has a problem of scale, it generally relates well to the large single family houses on the block.

When brick is used in new construction, the color should be in the red range, not yellow, gray, or salt and pepper. Further, when it is used as a building material, it should follow the structural requirements of masonry bearing walls, and not be suspended above ground.



No

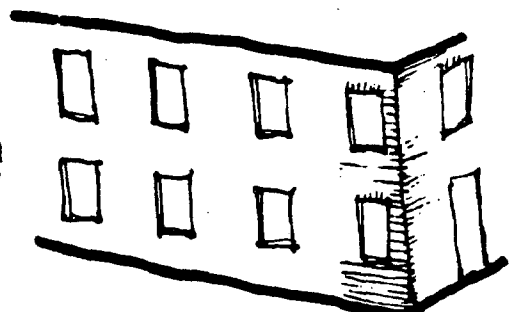
← Not like this
or this →



No

fig. 44

It was never used to build a structural frame of columns and beams. But was used to form load bearing wall with "punched" openings and larger solid structural piers.



b. Facade and Roof Colors

The prevailing principle in color selection should be to show architectural good manners to your neighbors.

While some Victorian-era color schemes are now considered flamboyant, even those were planned to fit into the context of their neighborhoods. The details and trim in small areas can still show some bolder colors as long as the overall surface treatment is more subtle.

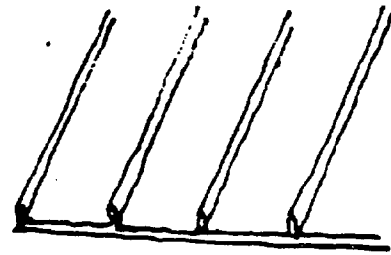
Although earth tones were popular in late 19th Century America when Solomons was thriving as a fishing and boating center, all of the buildings were probably not the muted browns, tans, and grays of that family. Dark green and blue were fairly common as siding colors with trim generally black or darker shades of the siding color.

Generally, the use of more than two colors on structures detracts from the appearance. However, some skilled designers can make multicolored schemes work, and the Review Committee will have to judge on a subjective level, the appropriateness of a proposed color scheme depending on the context.

Since the higher pitches of roofs on Solomons make these roofs more visible from the street and water, care must also be used in selecting appropriate roofing materials. While wood shingles and metal roofs were probably original, asphalt shingles can be used if the colors are chosen in the dark grays and browns, and are selected with an eye toward the building colors.

Roof materials were typically

- lead or copper (early)
- tin, standing seam (later)



standing seam

wood shingles
slate shingles

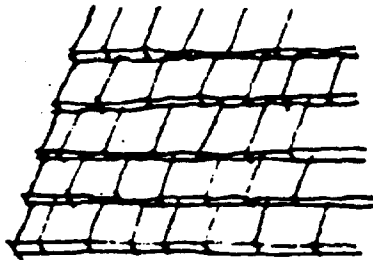


fig. 45

c. Window Details.

Windows help establish the basic character of a building and the glass divisions as well as the surrounding trim are details which either assist a building in fitting into its surroundings or make it stand out as inappropriate.

Windows were treated as openings punched into the flat planes of the walls. There is a typical proportion and rhythm to their placement. Window widths were typically in the range of 28" to 30". Window height is usually twice the width. Spacing between windows was usually equal to the window width, and up to twice that width.

Appropriate sash for this area is double-hung, (or single-hung in new windows), with a two over two pattern of mullions as shown in the illustrations below.

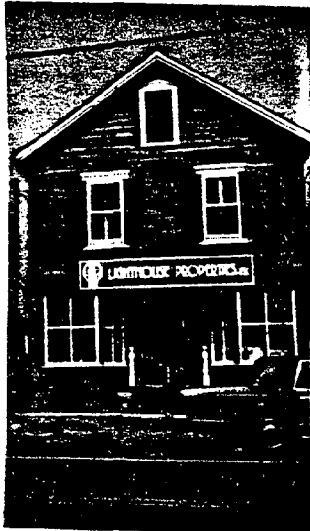


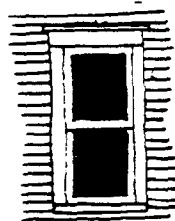
fig. 46

SOLOMONS MASTER PLAN

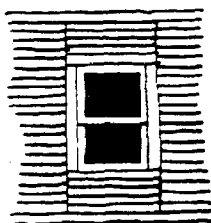
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Windows in Solomons

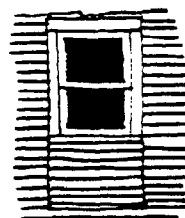
Right and wrong examples of window treatments are shown below, both in regard to re-sizing a window (which is extremely difficult to carry off successfully), and in replacing an existing window with a new model.



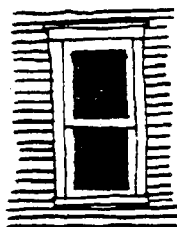
YES



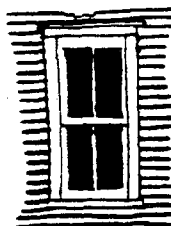
NO



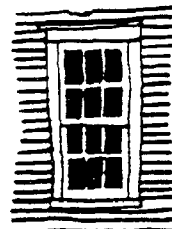
NO



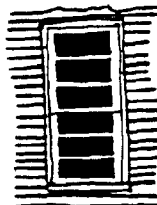
YES



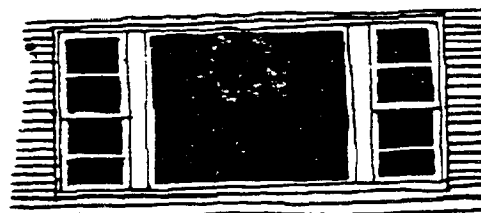
YES



NO



NO



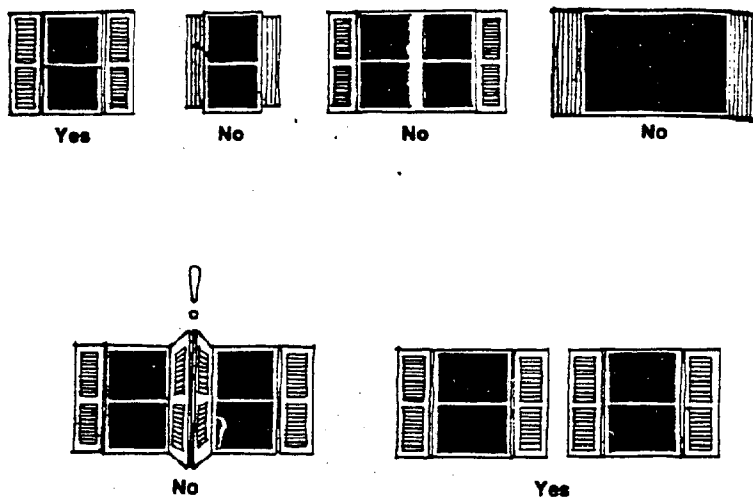
NO

fig. 47

The "modern" window treatment as shown below doesn't belong in a renovated building or in new construction which hopes to be compatible with and respect its historic context.



A glance at the older photos on the preceding pages will show very successful uses of shutter and trim around windows. The prime hint in using them is to make them look as if they could, if closed, cover the window. Even if the shutters are fixed in the open position, they should be sized right. The following examples will illustrate this principle.



fg.48

d. Doors and Entryways.

Door and entry treatment in the historical buildings were distinguished by several characteristics. Transoms over the doors were a very common feature as were the double glass sidelights on either side of the entry door.

Trim and moldings were not very ornate, but were generally built up of flat molding strips to give some bulk to the surroundings and frame the door openings. The door itself generally had one large glass light in the upper half and several recessed panels below.

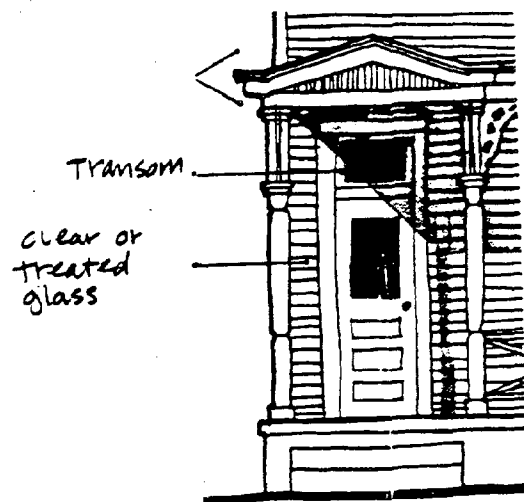
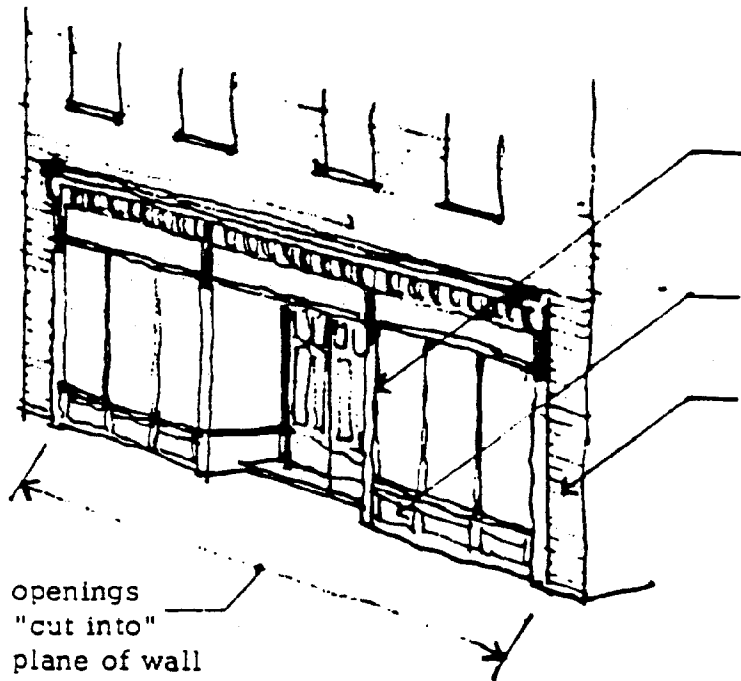


fig. 49

When a large window was needed, for example a show window in a store, it was designed like this:

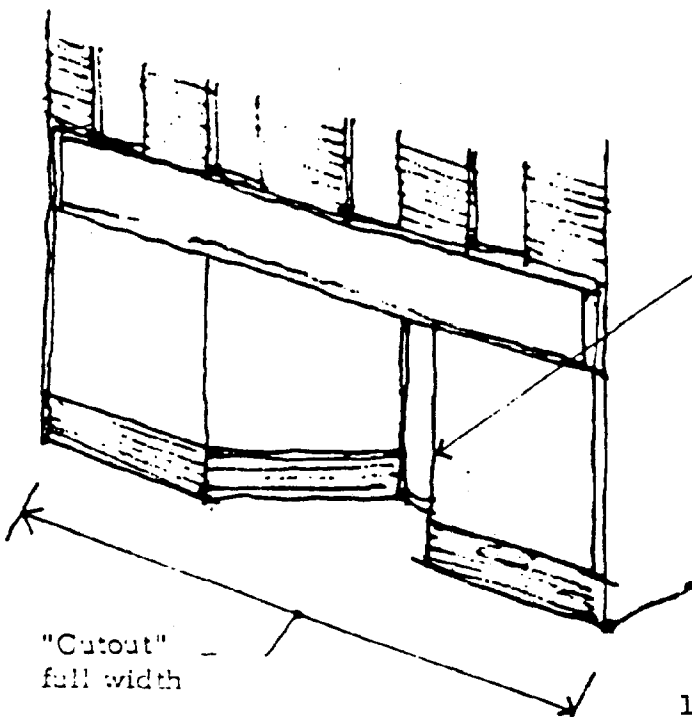


windows designed as frames and panels, with proportions and rhythms related to windows above

spandrel was part of window, not part of wall

wall meets the ground

openings
"cut into"
plane of wall

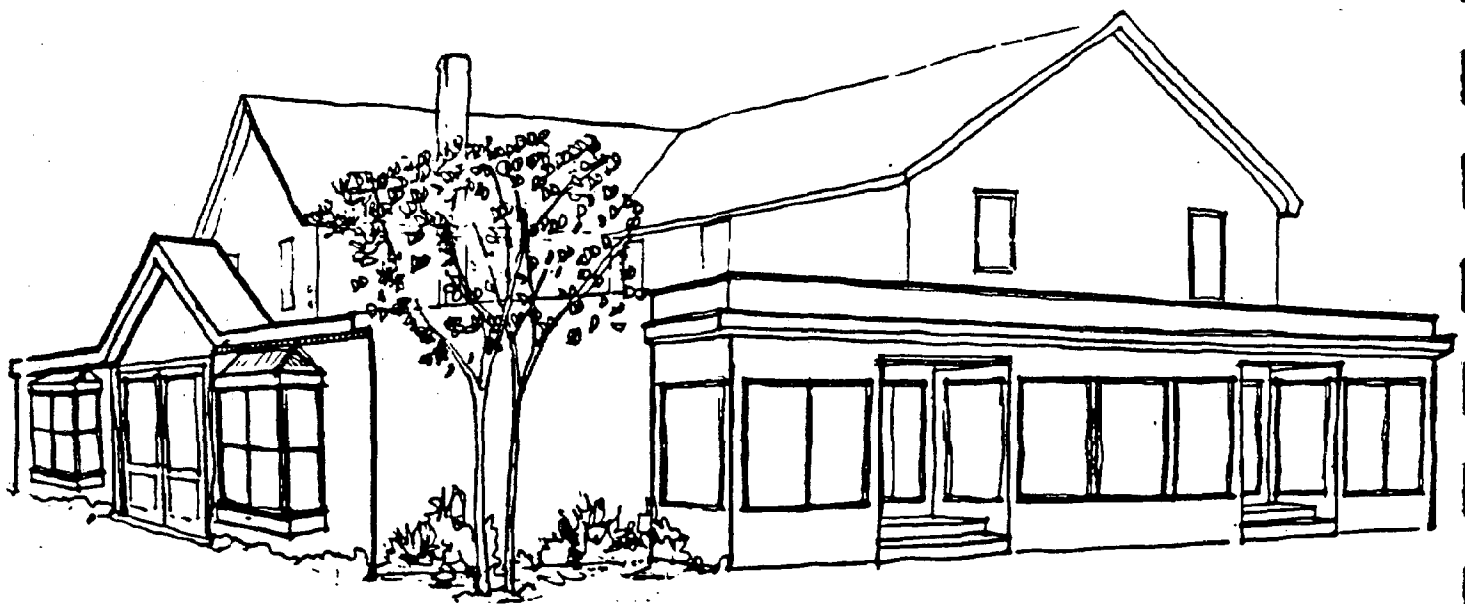


not like this

windows designed as a single plane with subdivision minimized

"Cutout"
full width

fig. 50



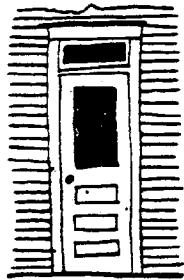
One specific site for remodeling was suggested at Woodburn's Market on Solomon's Road in Avondale. A new side entrance facing the parking lot was designed with a peaked roof which calls attention to the entrance and relates to the older store-fronts in the historic area.

fig. 51

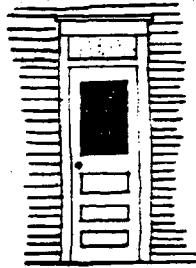
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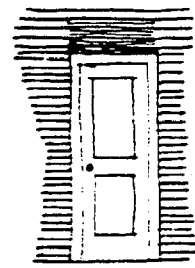
Woodburn's
Renovation



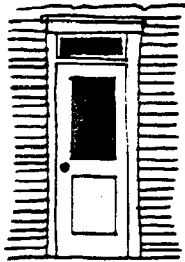
YES



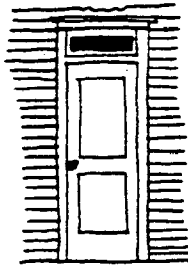
MAYBE



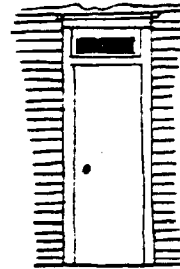
NO



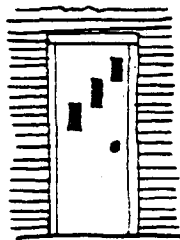
YES



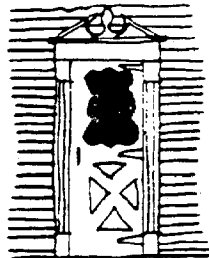
YES



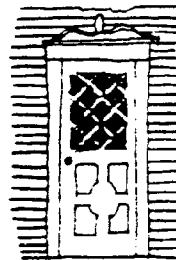
MAYBE



NO



NO



NO

fg.52

The above illustrations show some examples of renovation techniques both successful and unsuccessful in dealing with entry doors.

e. Ornamentation.

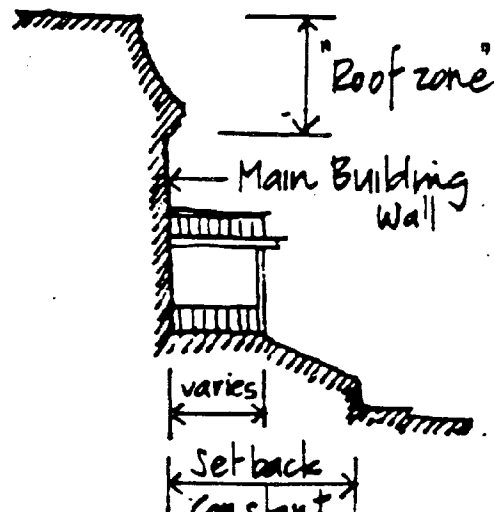
Eave and entry ornamentation are not very common on Solomons except for some notable examples at the Churches and some of the large Victorian houses south of Lore Street on Solomons Island Road.

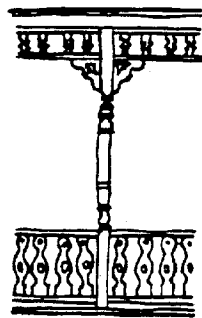
The primary ornamentation seems to be in the construction of porches, with turned columns, highly decorated brackets, and delicate balustrades. These features have been applied successfully to fairly simple house types with a very pleasant result.

In an area where porches are characteristic of the street, several rules should be followed:

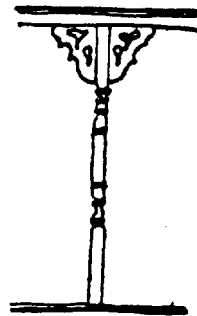
- o there should be a constant setback of the main wall,
- o all roofs should remain within a "roof zone"
- o variations of building colors, but similar trim and porch colors,
- o window proportions, details and landscaping should be similiar,
- o porches should be incorporated in the design.

The following illustrations show examples of early Solomons decorated porches, along with some guidelines for replacing porch columns.

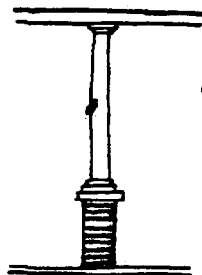




YES



YES



NO



NO

fig. 53

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Porch Ornamentation

f. Rooflines and Pitch.

A review of the old photos taken in Solomons and a walk around Avondale and the commercial district will show the preponderance of steeply pitched roofs on the Island.

Dormers were frequently used to allow habitable space in the roof area, and they provide an attractive feature on the older buildings.

One example of a turreted roof exists on Solomons Island Road, and a comparison of the present day appearance with a photo taken during construction shows that the house has retained all of its original charm.

Roof pitches should be in the range of 10/12, or ten feet of vertical rise in twelve feet of horizontal run. This would allow a roofline compatible with most of those existing in the historical period. Some dormer roofs are flatter, but the overall appearance should be of a rhythm among the structures in an area. The following photos and drawings illustrate some typical Solomons examples along with additional buildings appropriate to the overall design guidelines.



fig. 54



Hotel



fig. 55

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**Rooflines in
Historic Solomons**

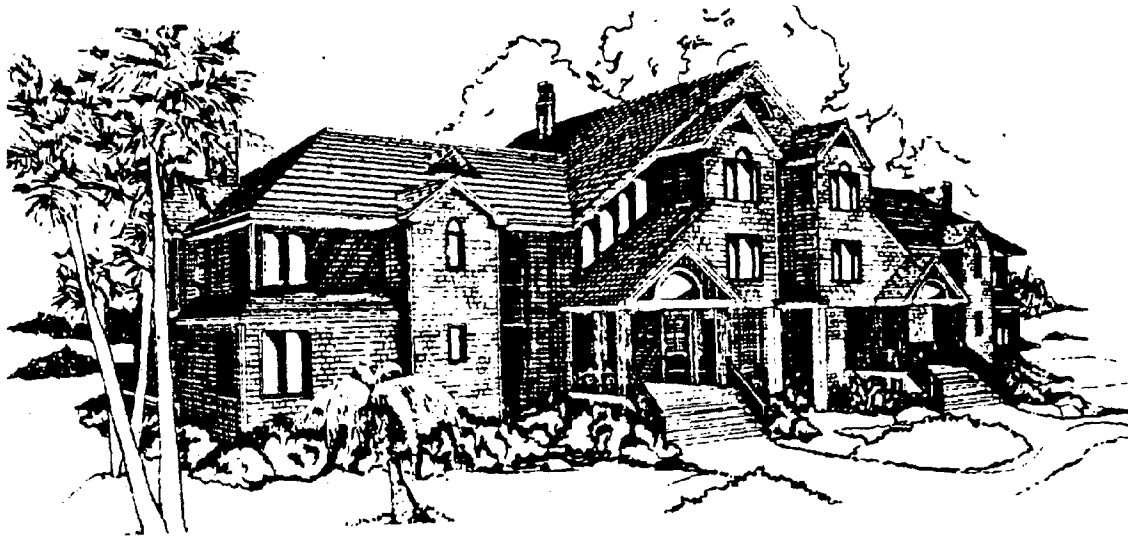


fig. 56

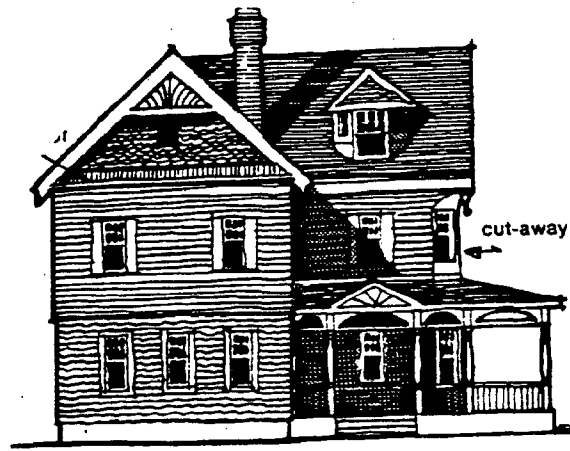


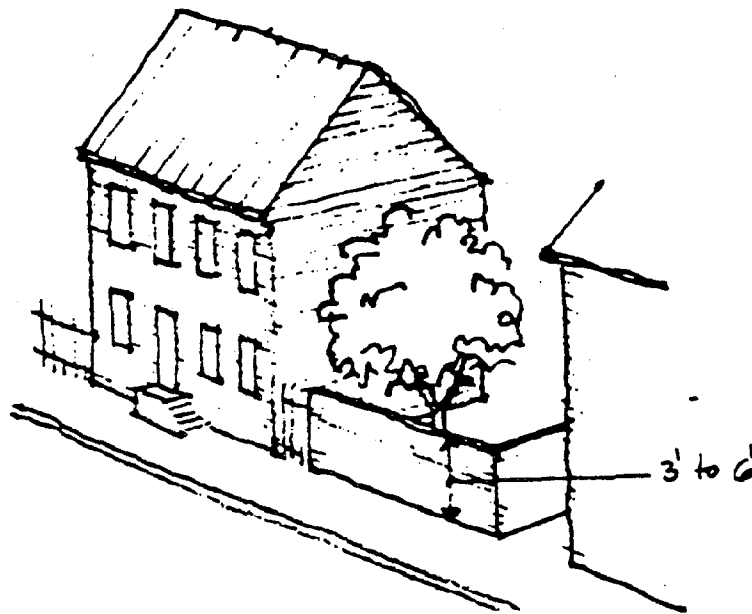
fig. 57

2. Treatment of Accessory Structures.

There is only one important point in design or renovation of accessory structures such as sheds, screen walls or fences. They should not detract from the original building, and, in fact, should be designed to complement or even enhance the design of the main building. They should conform with the general material and color as well as the character of the existing building.

a. Fences

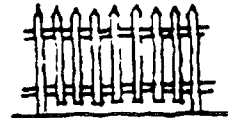
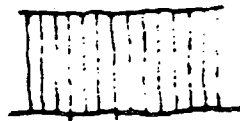
Often garden walls and fences were used for privacy, and they contributed to the continuity of the streetscape.



Wood

solid
plain boards

or with spacings
like picket fence



not patterned or
diagonal boards

not uneven tops or
stockade

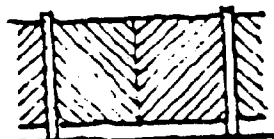
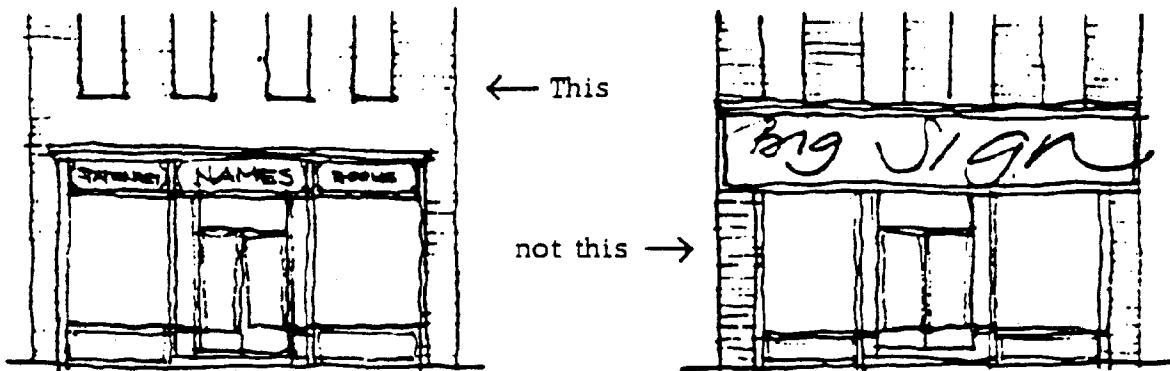


fig. 58

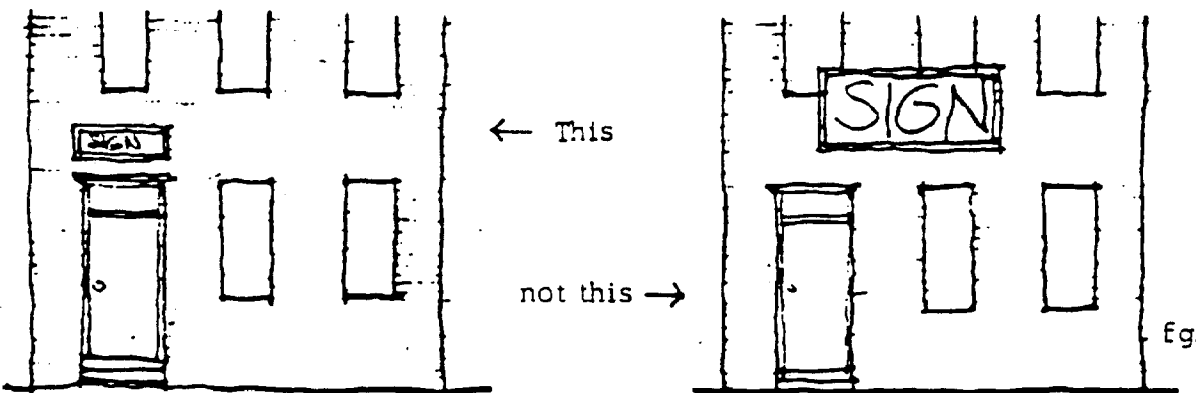
b. Signage.

When signs were added to buildings, they were made part of the overall architectural composition of the buildings.

The basic building block with its corners was maintained.



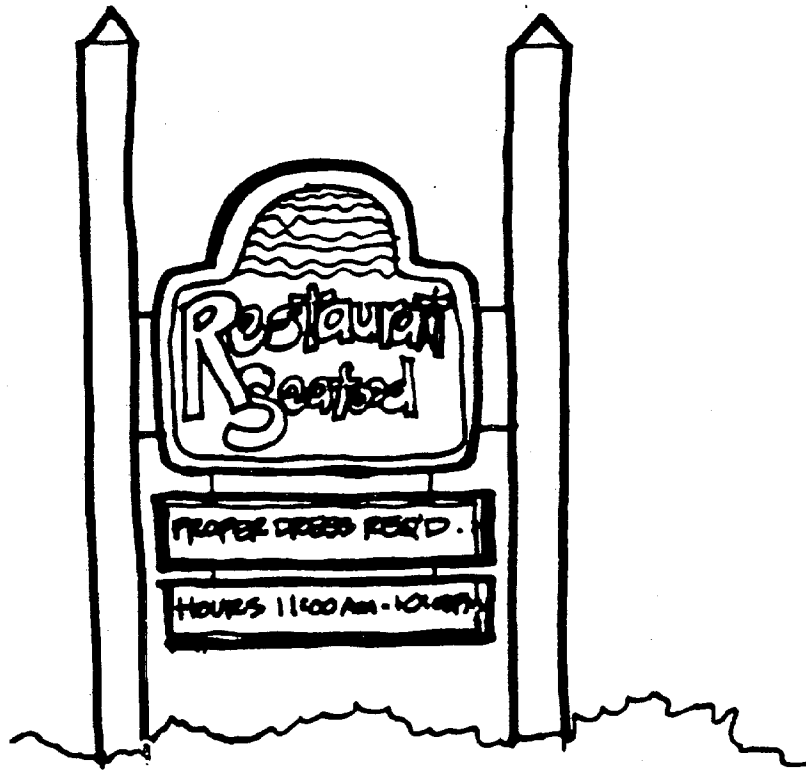
- Signs were in scale with building elements and complemented rather than overwhelmed the building.
- Signs were related to architectural features of the facade, usually the entrance



Eg.59 -

Signs were designed to be read by pedestrians, and not for people driving past at 30 miles an hour.

Materials: painted wood Not neon/and or back lighted



fg.60

There is a need for controls on commercial signage to eliminate a great deal of visual clutter we're faced with on a walk through Solomons. The zoning ordinance is quite specific as to the allowable size of signage so the only guidelines are related to shape and materials. These two drawings are some examples of appropriate types of commercial signage.

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Private Signage

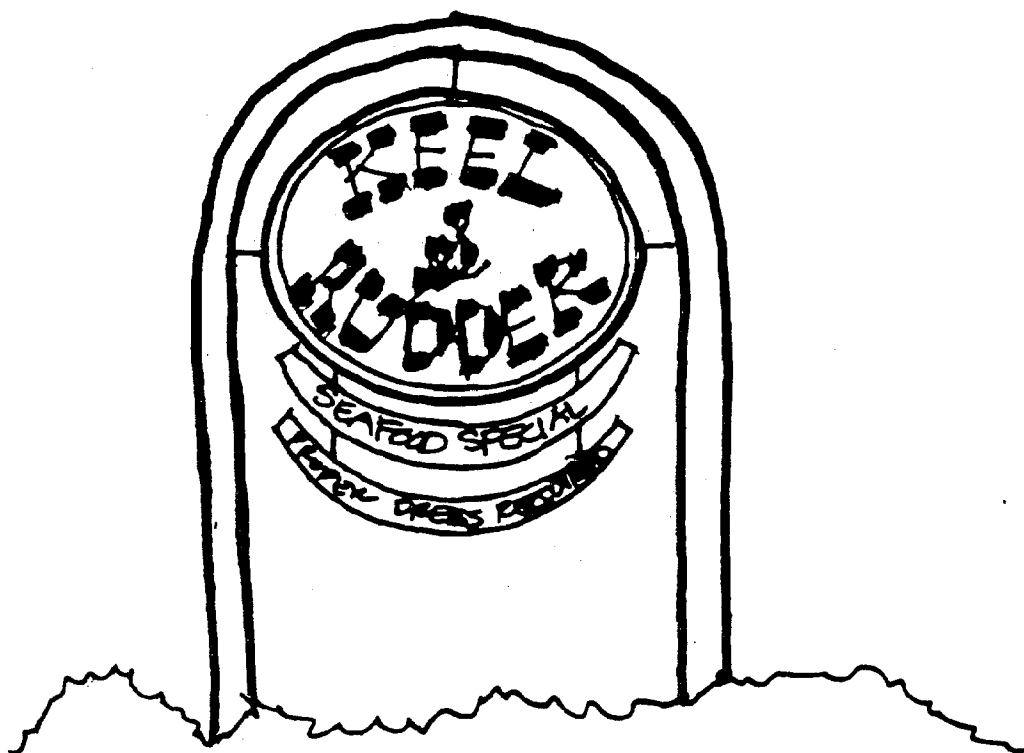


fig. 61

Wood is the preferred material, although it should be painted in the same types of colors discussed for buildings. Some use of brighter colors is accepted here, but only in highlight areas. Only indirect lighting is permitted, and the light housing should be hidden.

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Private Signage

c. Lighting.

Exterior lighting should be informative, yet not competitive with the surrounding lighting.

Night use of lighting on the exterior or for display is encouraged for both safety (of person and property), and as a spot advertisement for specific goods within the window. These lights should be simple and informative, creating a pleasant illumination of the evening street scene.

Some general rules are:

- o exterior lighting should be limited to lighting fixtures designed to be in harmony with the character of the building and Solomons. Such fixtures should be mounted in the entrance ways and on the front facades of buildings.
- o Flood lighting concealed above a storefront roofing may be used to light the facades of buildings.
- o fully recessed downlights or wall washers in projecting metal boxes which run the full length of the sign area are permitted, as are shielded fluorescent lamps with diffusers.
- o gooseneck incandescent, porcelain enamel reflectors on bent metal tube arm is also appropriate.
- o lights should not blink, black out, flash, or have any mechanical motion.
- o electrical elements such as wires, conduits, junction boxes, transformers, ballasts, switches, and panel boxes should be concealed from view.

d. Rear and Side Yards : Trash Receptacles.

Yards should be neat in appearance and should not in any way obstruct passersby or detract from the appearance of the building.

If a side or rear yard is used for parking, it should be effectively screened from adjoining residential property and should have adequate landscaping and screening on property lines adjacent to public sidewalks.

All outdoor refuse storage areas should be screened from the view of adjacent properties and the public right-of-way. Screening could consist of an appropriate fencing material or a compact evergreen hedge of not less than 4' in height at the time of original planting.

#

The following sources are gratefully acknowledged:

1. Calvert Marine Museum Photographic Archives.
2. George Stephen, Remodling Old Houses Without Destroying Their Character, Alfred A. Knopf, 1973.
3. Design Guide for the Exterior Rehabilitation of Buildings in Old Anacostia, School of Architecture, University of Maryland, 1975.
4. Annapolis Historic District: Design Guidelines for New Construction, Robert Lamb Bart, 1978.
5. Design Guidelines Handbook, Montgomery County Maryland, 1979, Komatsu/Brown Architects.
6. Seabrook Island Promotional Information, Seabrook Island, Georgia.
7. Appearance Review Enabling Ordinance, Calvert County, Md.

CHAPTER VI

PUBLIC IMPROVEMENTS

CHAPTER VI

PUBLIC IMPROVEMENTS

A. Introduction

B. Public Improvements

1. Streetscape
2. Solomon's Entry
3. Bulkhead Along the Patuxent
4. Commercial Pier Along Patuxent
5. Recreation Information Center
6. Bulkhead Along Back Creek
7. Sidewalk
8. Bicycle Path
9. Undergrounding Utilities
10. Museum Parking
11. Shuttle Bus Service
12. Farren Avenue Turnabout

C. Traffic Management

1. Bridge Access Intersection Improvements
2. Street Signage
3. Dowell Road
4. Traffic in Planning Areas B & C

D. Parking

1. Public Parking near the Johnson Bridge
2. Public Parking for Trailers
3. Bulkhead Parking

E. Recreation and Parks

1. Solomons Center Park
2. Molley's Leg

A. INTRODUCTION.

One of the most important opportunities to improve the visible appearance of the Solomons area lies in improvement of the storefronts, implementation of appearance guidelines, and in the creation of a pleasant pedestrian environment.

For these improvements to work to their maximum effectiveness, they should be carefully designed and closely coordinated. The exterior of building facades is an extremely important component. Because some of the buildings have been altered in a variety of ways, particularly over the past 30 years, removal of incompatible elements and enhancement of the original ones underneath, as discussed in the previous chapter, can be an inexpensive and dramatic way of improving the visible appearance of Solomons.

A second critical component is the public improvements. Over the years, the quality of the public improvements, that is, the sidewalks (where they exist), curbs and gutters, trash cans and paving has suffered serious deterioration. Lack of maintenance, particularly on the bulkhead, and inadequate pedestrian amenities and trees have created a sterile and barren appearance in the historic commercial area.

Any public improvements plan must take into consideration the present routes which vehicles and people use as they move around the area; the relationship between present and proposed architectural structures; and the colors and textures of the small-scale features such as proposed landscaping, paving, sidewalk materials, street furniture, and graphics which - during the day or at night - create the total visual experience of the area.

B. PUBLIC IMPROVEMENTS.

1. Streetscape.

A streetscape includes all the area between the building fronts on one side of the street to the building fronts, or in the case of Solomons, the bulkheading on the other side. This includes the sidewalk, the curbing, the parking meters, benches, pavement, trees and grass, trash receptacles, rest rooms, traffic signs, street lights and public signage.

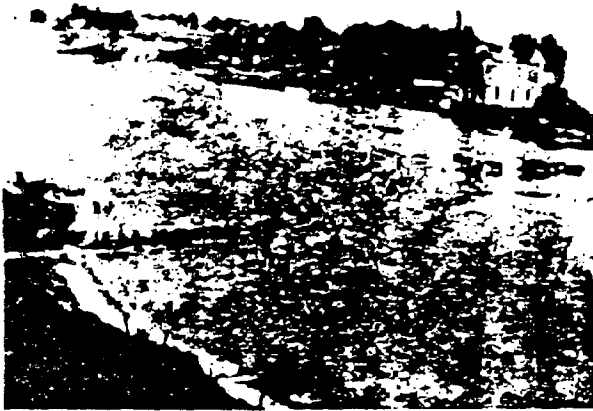
The basic design of public improvements should serve to highlight the main entrances and intersections of the commercial area, to create distinctive vistas along the main streets, and to provide amenities such as rest areas and green spaces that reduce the harshness of building materials and concrete paving. The effect of such improvements should be to create a sense of place which encourages consumers and tourists to shop, to stroll, and to enjoy the area.

A look at the existing streetscape immediately visible at the "entry" to Solomons Island gives a good indication of some of the major problems with the area.



fig. 62

As seen in the photo above, the main orientation is to the parking lot. Cars and surface paving predominate at the natural "gateway" to historic Solomons. This is distinctly unlike the earlier entry to Solomons which, before the causeway was filled in, was sandy shoal and in 1870 a wooden footbridge 550 feet long.¹



The Narrows before the causeway was filled in. The Solomon's Methodist Church can be seen at right. (Photo courtesy of Doris Woodburn Johnson)



Johnstown with first vehicular wooden bridge to Solomons on right. (Photo courtesy of Calvert Marine Museum from a postcard dated 1909)

fig. 63

The first vehicular bridge to Solomons was completed in 1907. The current "tide box" bridge resulted from the progressive widenings of the road in 1918, 1948-9, and again in 1957.

¹R. Eschelman, Historical Tours, p.11

Because of the gradual dominance of the automobile as the principal means of entry to the historic commercial area of Solomons, there has been little done to ensure a separation of modes of travel, such a walking, bike riding, and car. It is a principal concept of these proposed public improvements, that there be a clear and attractive separation of these modes.

In order to improve the streetscape and functioning at the entry point to the historic commercial area, this Master Plan proposes the following type of transportation separation on the Patuxent River side of Solomon's Island Road.

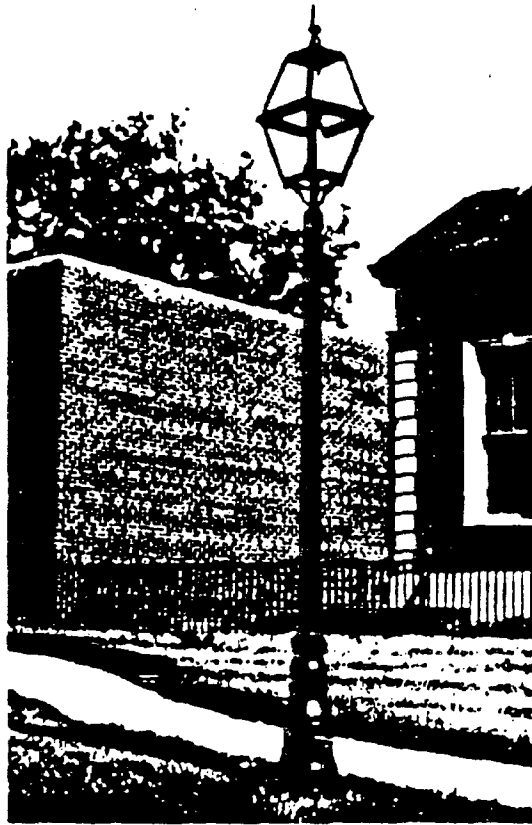
As illustrated in the drawing below, a continuous landscaping strip should separate the street and the bike land from the parking lot. The shrubs in this buffer strip should be in the range of two to four feet high. At this height, the strip should begin to block many of the parked cars from view.

Shade trees above will provide both a change of scale and protection at the street level, and some visual interest along the entire commercial strip.

The additional lane on the river side of Solomons Island Road, now used as a temporary parking area or a turning land for entry to the parking lot is proposed as a bike lane. This lane is proposed to continue throughout the Town Center area. (See Figure 78).



fig. 64



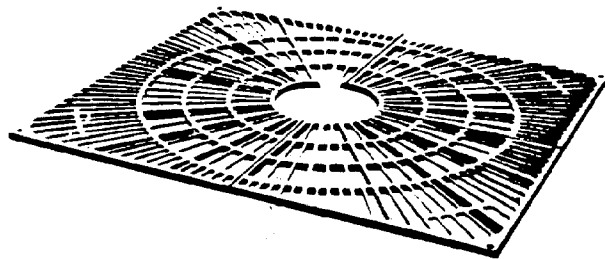
965-P on 3608-P post features 14½x37" scale lantern with frosted chimney carrying 100 watt mercury vapor or 70 watt high pressure sodium lamp and our seeded acrylic lens. Available in 175 watt mercury vapor or 100 to 150 watt high pressure sodium.

Fig. 65

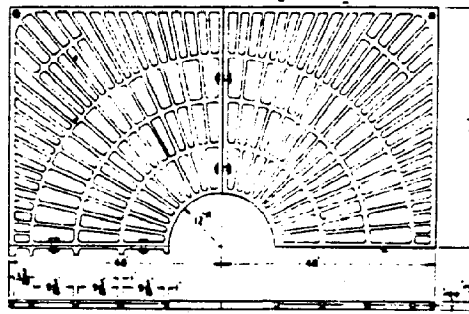
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STREET LIGHT



Light duty design requires four sections for complete unit.
Support at section joints is required, and is to be furnished by
installer. Tree opening is expandable.
Weight per set - 1250 pounds.



HALF PLAN AND SECTION

fig. 66

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Tree Grate



fig. 67

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Current
Streetscape

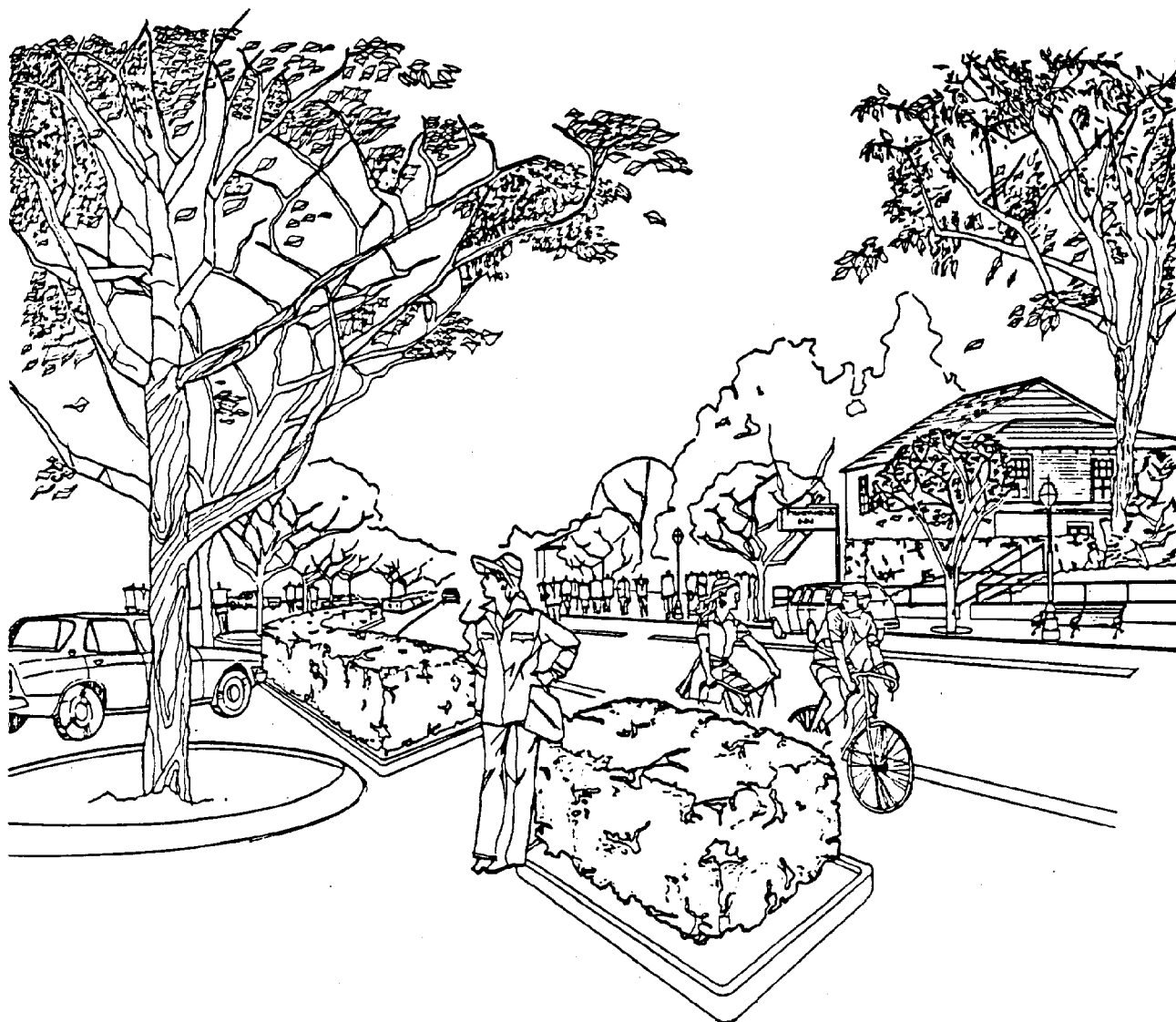


Fig. 6E

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**Proposed
Streetscape**

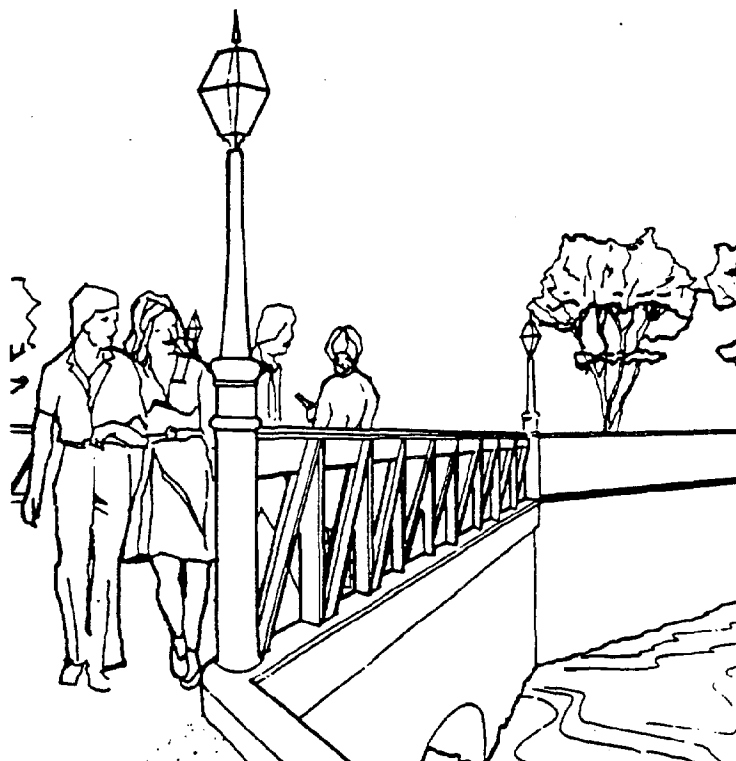
2. Solomon's Entry.

Few first time visitors to Solomon's Island know when they have entered the "island" area. The historic separation of island from the peninsula is no longer obvious to the casual observer.

Recommendation:

In order to begin to recreate a "sense of place" in historic Solomons and to provide an attractive, functional and historic entryway, this Master Plan proposes the construction of a more prominent and attractive bridge at the "tide box".

The following illustration attempts to recapture some of the steel/cast-iron feeling of bridge construction popular at the end of the 19th century. Reconstruction of the original wooden type of structure would be both impractical and undesirable.



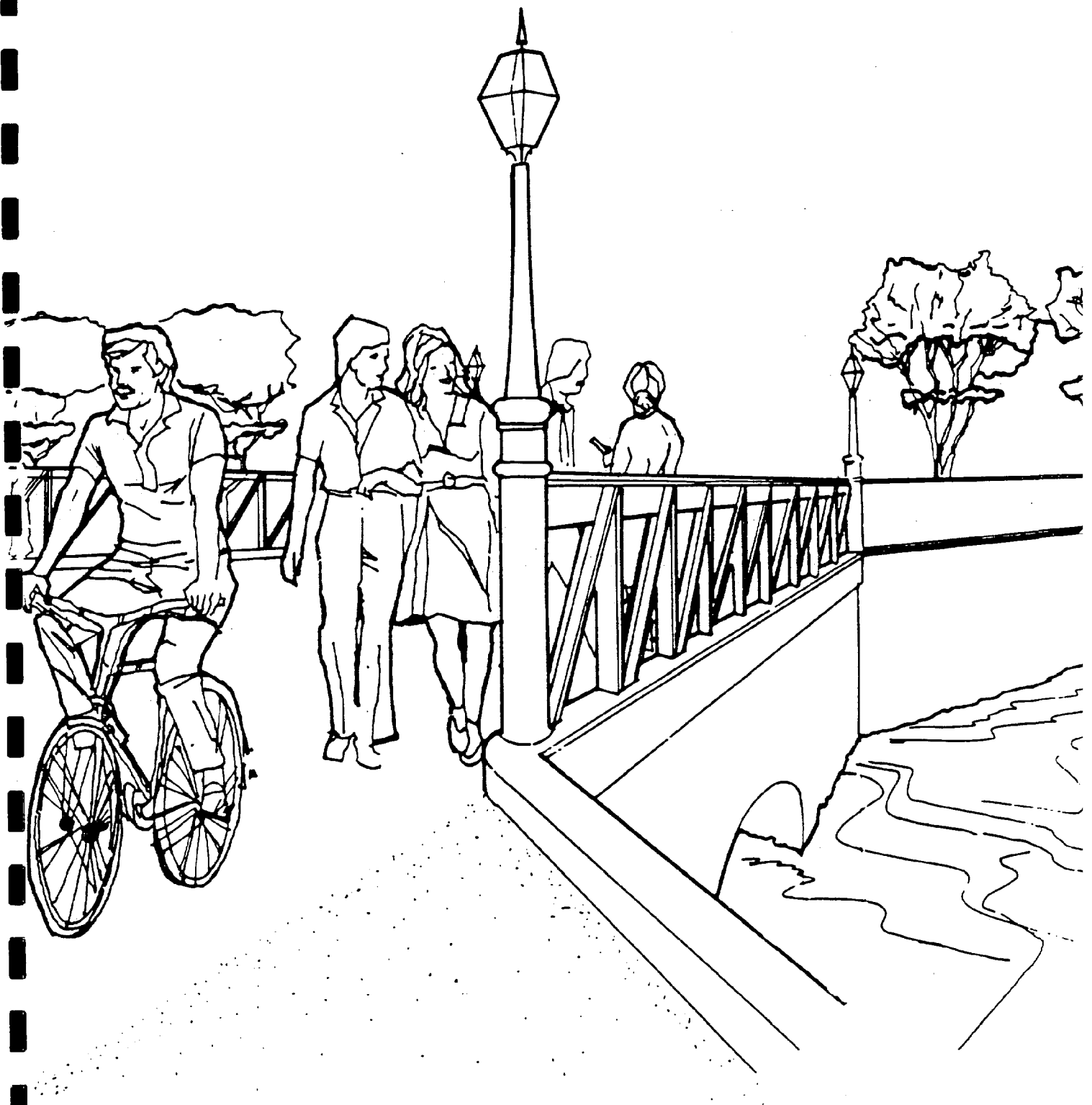


fig. 69

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ENTRY BRIDGE

3. Bulkhead Along the Patuxent.

The bulkhead along the Patuxent River has, in recent years, has acted as an "attractive nuisance" at the heart of the historic commercial area. While providing parking for many of the customers which visit the restaurants and shops along Solomons Island Road, the bulkhead also has attracted overnight campers, boat trailers, and fishermen - adding to the problems of maintenance and policing. In addition, the only public restroom facilities are temporary rental structures located on the bulkhead at the entryway to Solomons.

Recommendation:

The bulkheading from the entrance of Solomons Island to Solomons Pier Restaurant should be expanded. An additional bulkhead south of the Restaurant should be constructed.

Through careful design, this expanded bulkheading would provide additional parking, and a pleasant tree-lined walkway for pedestrians along the River.

Parking would be restricted to automobiles only, with the following single exception. Five transient boat trailer parking spaces each should be provided for the two existing businesses: H.M. Woodburn's and Dockside. Their use of these spaces would be restricted until the business or property changes hands or changes management or until 5 years after construction of the new bulkhead, at which period this exception should be reviewed and redetermined.

Fishing from the bulkhead, and overnight parking would be prohibited.

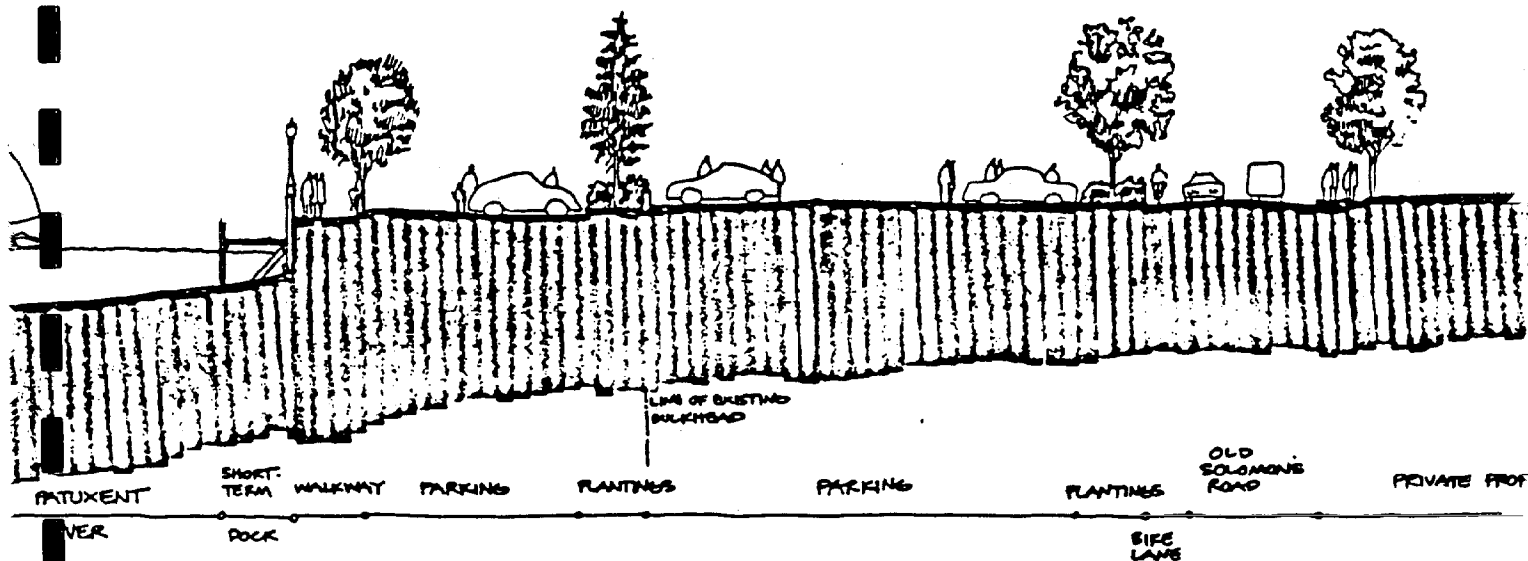


Fig. 70

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Expanded
Bulkhead

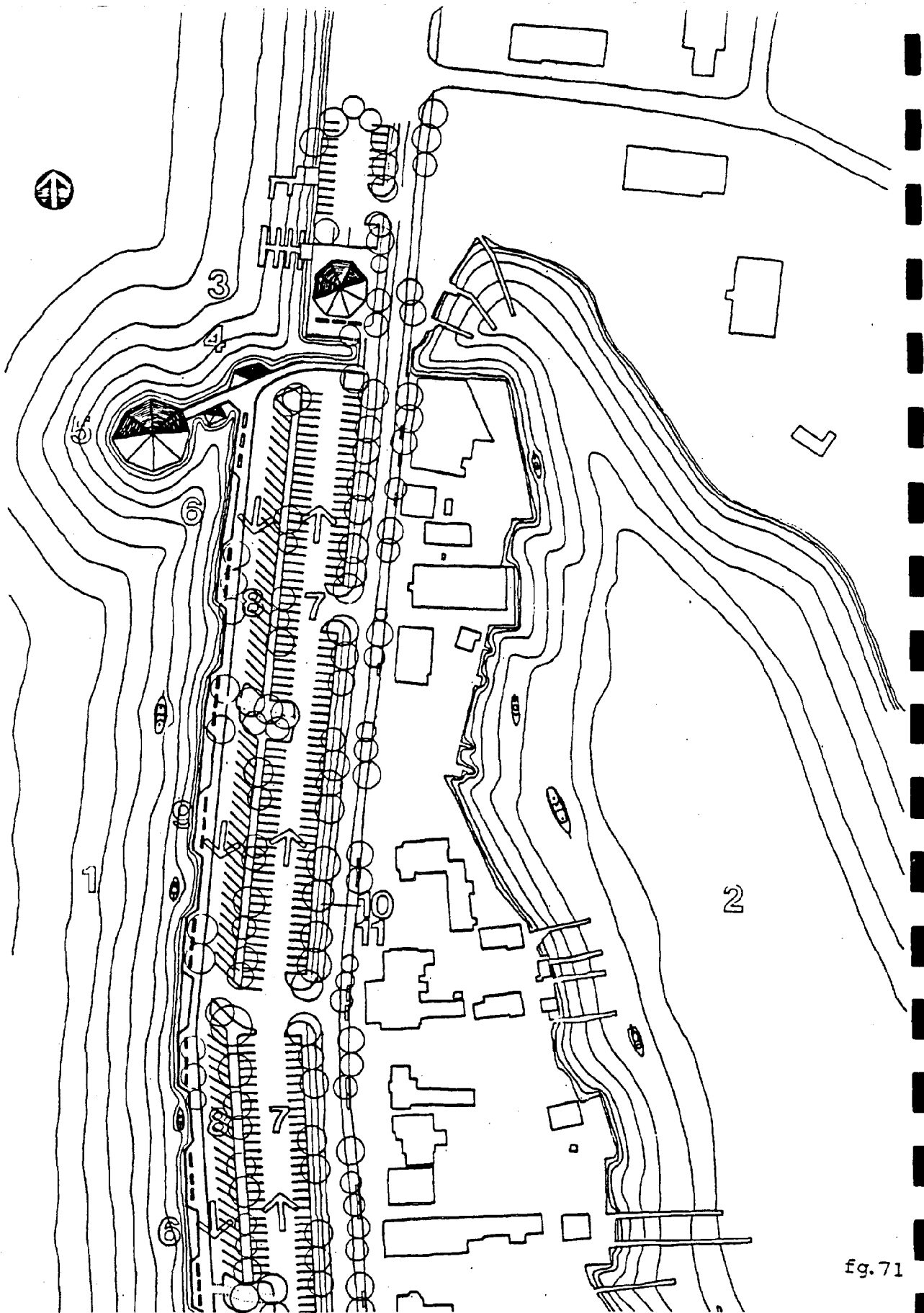


fig. 71

SOLOMONS MASTER PLAN

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TOWN-NORTH

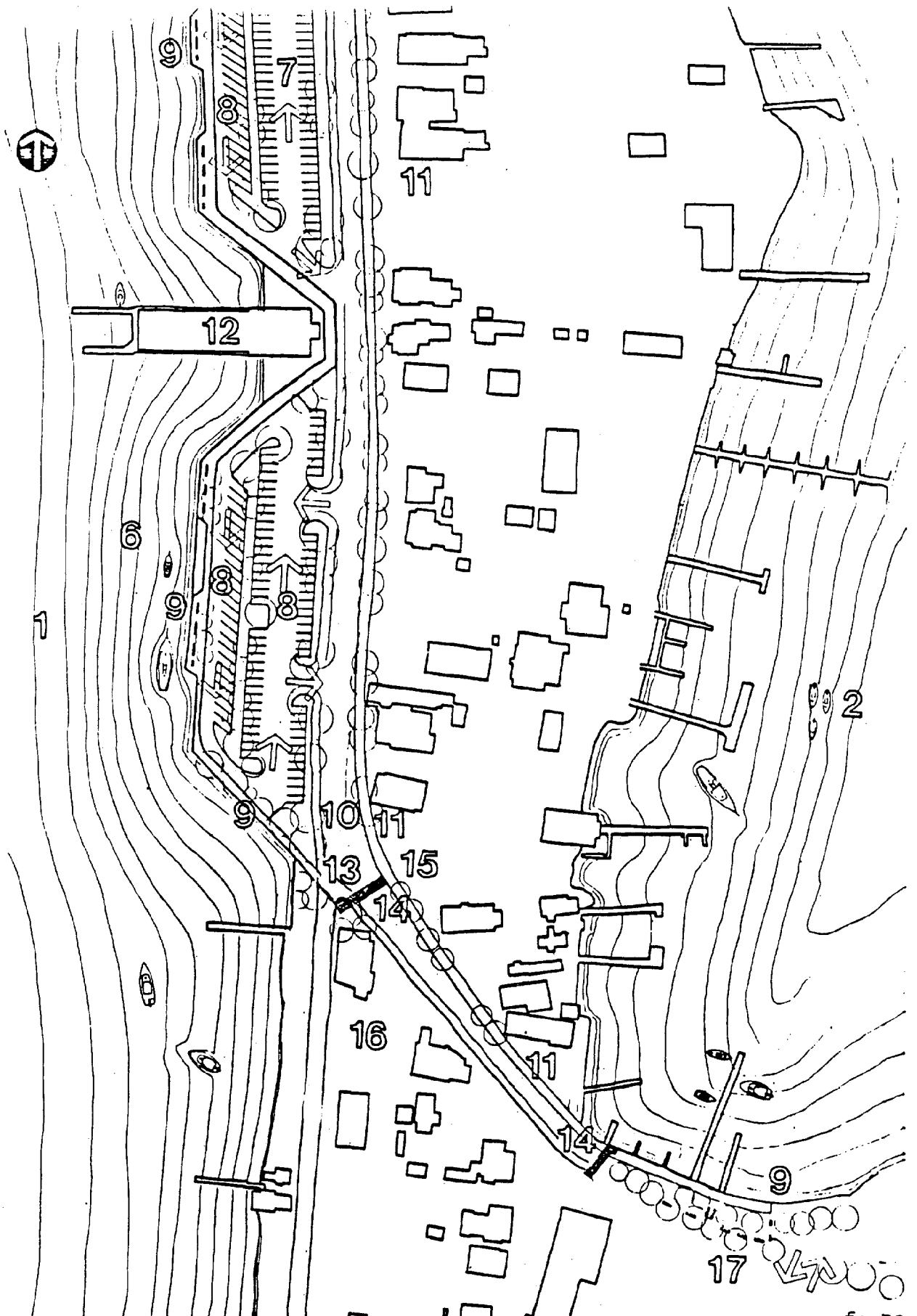


fig. 72

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TOWN-SOUTH

KEY TO MAPS

1. Patuxent River
2. Back Creek
3. Concession/Bike Rental
4. New Bridge
5. Shopping Pier
6. Transient Mooring
7. Existing Parking Area
8. New Parking Area
9. "Riverwalk" Pedestrian Boardwalk
10. Bicycle Lane
11. New Sidewalk (Exposed Aggregate)
12. Existing Restaurant
13. Private Entry to Sandy Point Community
14. Crosswalk
15. Realignment of Street Curve
16. Sandy Point Residential Community
17. Landscaped Seating Area
18. Shops

4. Commercial Pier Along Bulkhead.

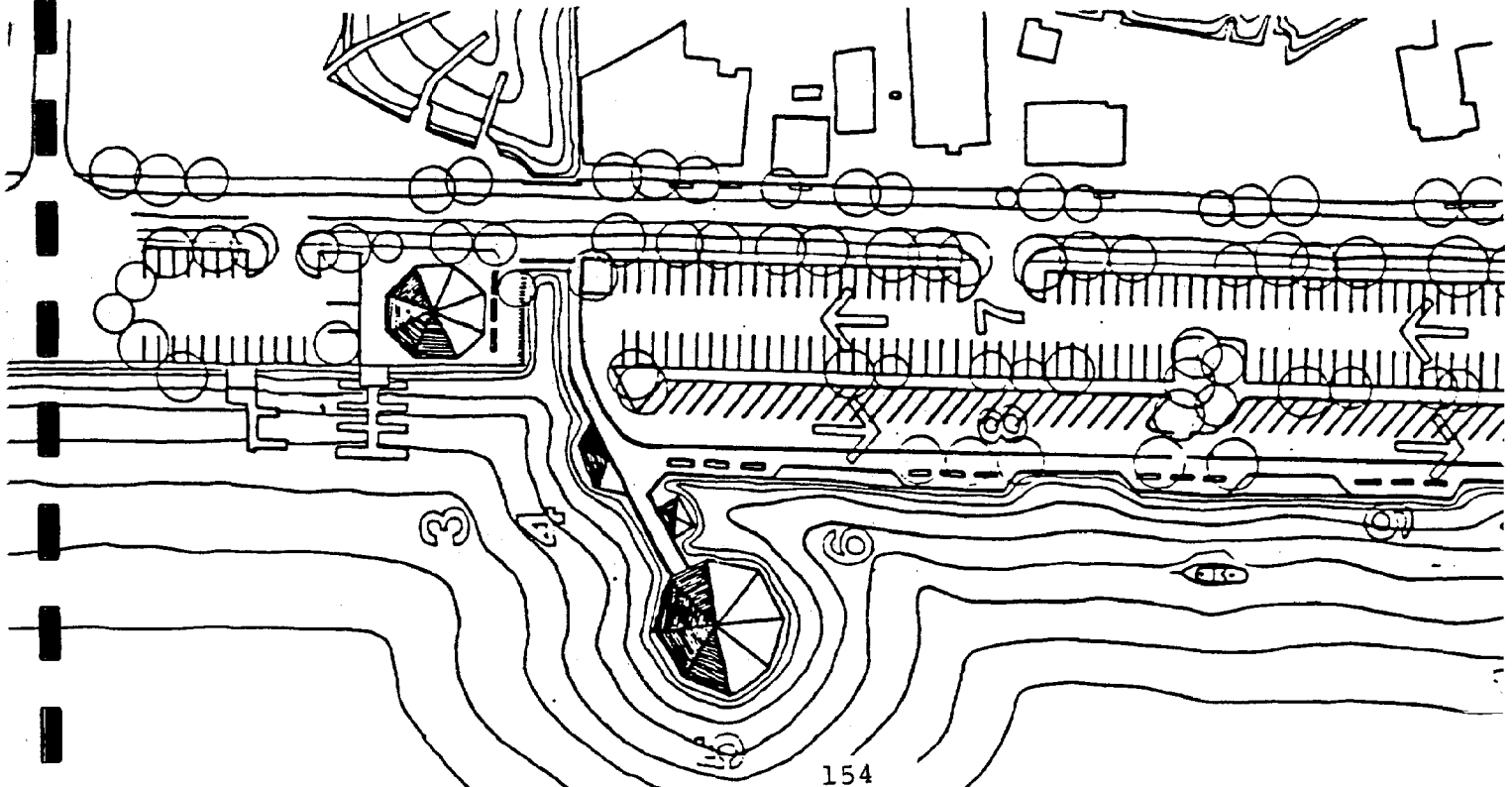
The historic commercial core of Solomons Island is both landlocked, and of very narrow land width. There is today both interest and pressure from those holding riparian rights on the Patuxent River to construct privately owned and operated piers along the bulkhead. It is both desirable and economically feasible to encourage the concentration of small scale retail along the water's edge. However, it is environmentally and aesthetically undesirable to permit the proliferation and duplication of private commercial piers along the public bulkhead.

Recommendation:

A single commercial pier, composed of a variety of shops, should be built out from the expanded bulkhead, near the entrance to Solomons Island, which consolidates the retail activity at the water's edge. A fresh seafood market may be incorporated into this area.

The County should negotiate with property owners holding current riparian rights on the bulkhead, in order to proceed with construction. An option may be to offer these owners first rights to retail space on the pier.

fig. 73



5. Recreation Information Center.

Solomons needs a facility which will provide public restrooms near the heart of the historic commercial core. In addition, both Calvert and St. Mary's Counties needs a public facility which would provide recreation and tourist information both to residents and visitors. Moreover, it would be extremely useful to have a bike rental facility near the heart of the commercial core and parking, so that visitors would be encouraged to leave their automobiles.

Recommendation:

Construct an attractive kiosk/pavilion on the current site of the pumping station to the north of the "tide-box". This station is scheduled for reconstruction in the County's public works budget, and during the course of reconstruction, the pumping station could be incorporated into the Information Center structure. Such a new facility should include: public restrooms, a bike rental facility, and a recreational information center. The small area in front of the proposed facility would provide short-term (30 minute) parking.

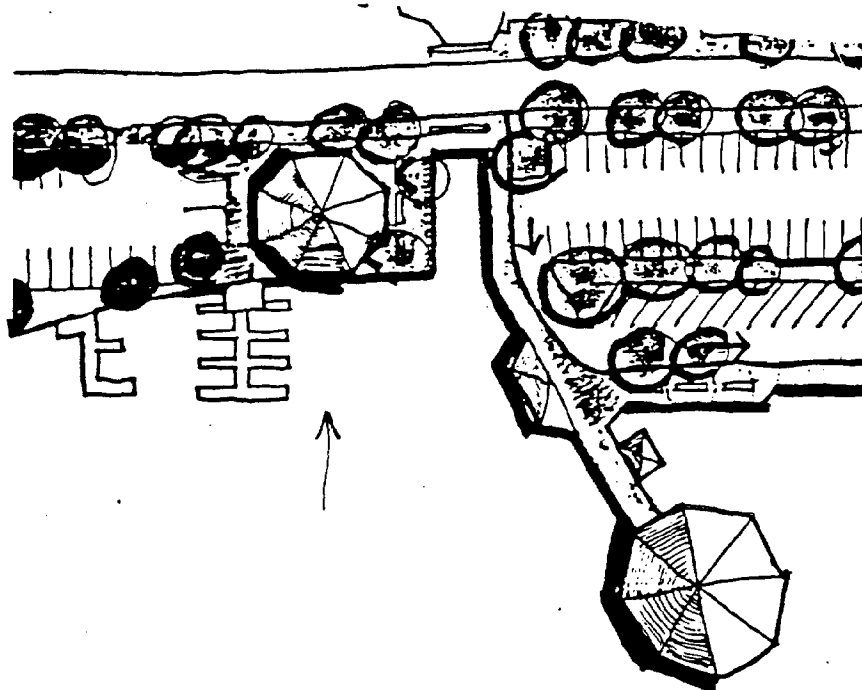


fig. 74

6. Bulkhead Along Back Creek.

The waterfront between Back Creek and Charles Road is of narrow land width and is not connected in any physical way with either the waterfront activities on the Patuxent River or with the historic commercial area along Solomons Island Road. This waterfront, held substantially by one owner, offers an opportunity to unite the waterfronts and provide an attractive public/private space.

Recommendation:

The new bulkhead area proposed for south of Solomons Pier Restaurant should be united, through a clearly defined walkway, with a new bulkhead along the Back Creek. The proposed bulkhead should begin where Charles Street and the water are the closest and continue to the end of the Harbor Lights property.

It is also recommended that a small attractive retail facility be constructed adjacent to the scenic walkway. This should not house the originally proposed fish market, but should incorporate a series of small shops, continuing the retail theme.

It is desirable that, if the harbor entrance site at the northeastern tip of the Island is redeveloped, this walkway continue around to the end of Charles Street.

The County should negotiate with the current owners so that a permanent public easement is acquired if the bulkhead and walkway are constructed with public funds.

In addition, the County should pursue the acquisition of waterfront easements from the other property owners along Back Creek, with the long-term intention of uniting all of Back Creek on the western side with a scenic walkway.

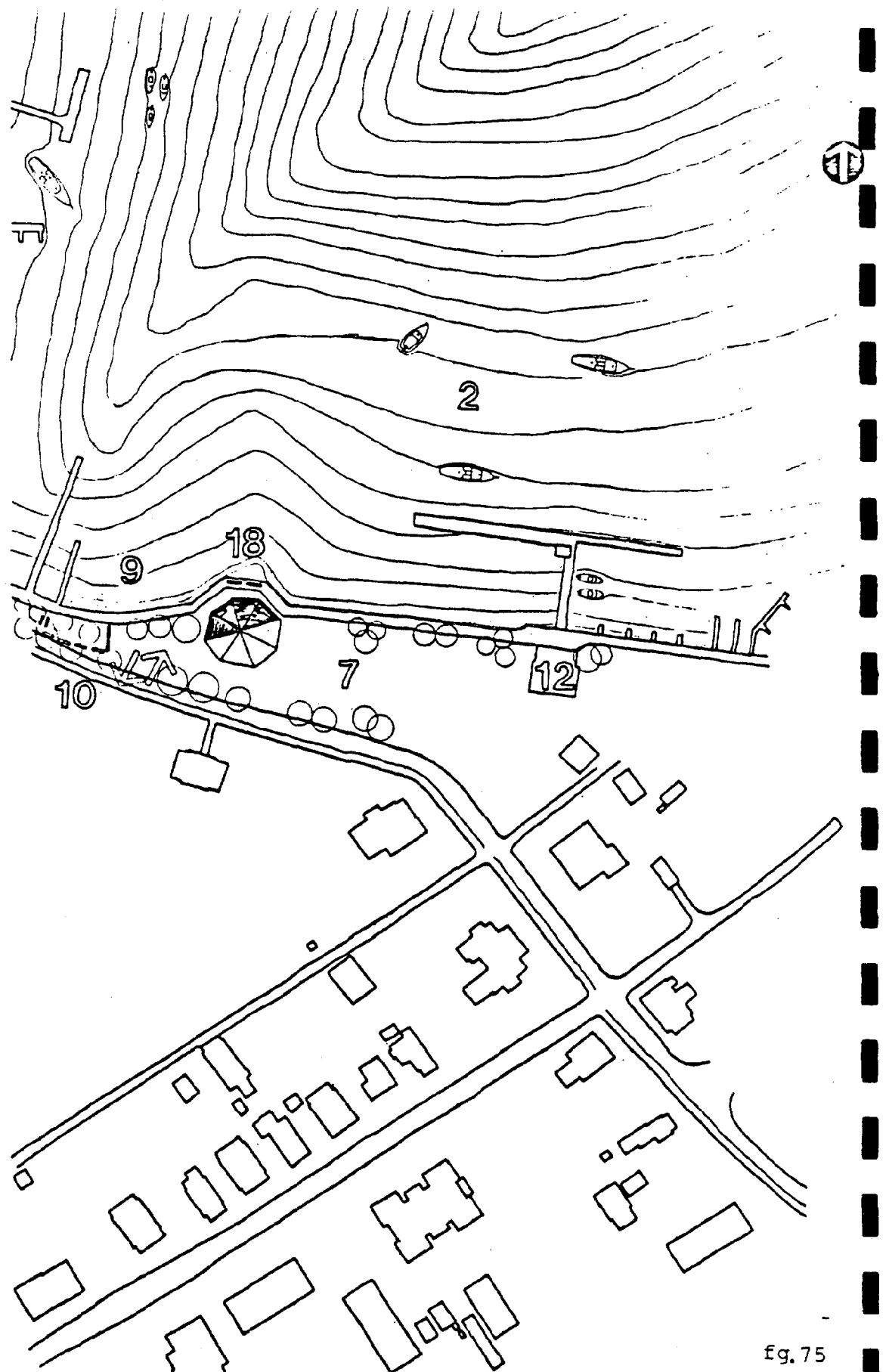


fig. 75

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TOWN-EAST

KEY TO MAPS

1. Patuxent River
2. Back Creek
3. Concession/Bike Rental
4. New Bridge
5. Shopping Pier
6. Transient Mooring
7. Existing Parking Area
8. New Parking Area
9. "Riverwalk" Pedestrian Boardwalk
10. Bicycle Lane
11. New Sidewalk (Exposed Aggregate)
12. Existing Restaurant
13. Private Entry to Sandy Point Community
14. Crosswalk
15. Realignment of Street Curve
16. Sandy Point Residential Community
17. Landscaped Seating Area
18. Shops

7. Sidewalks.

The sidewalks in the study area are in bad repair and, in many cases, do not exist. Not only does this condition constitute a hazard to pedestrians, but it leads to a poor image of the area, particularly in those sections illustrated on the accompanying map, where many receive their most lasting impression of Solomons.

In addition, a pleasant walking environment is needed to encourage museum and marina patrons to come on foot to the Island area.

Recommendation:

A sidewalk tying in the Navy Recreational Center with the proposed recreational vehicle camping area under the Thomas Johnson Bridge, the Calvert Museum with the scenic walkway on Solomons Island, should progress down the eastern side of Solomons Island Road from Lore Street to the intersection of Charles Street and Patuxent Avenue, crossing over Charles Street to the west side of Charles ending at the intersection of Charles and Farron Avenue.

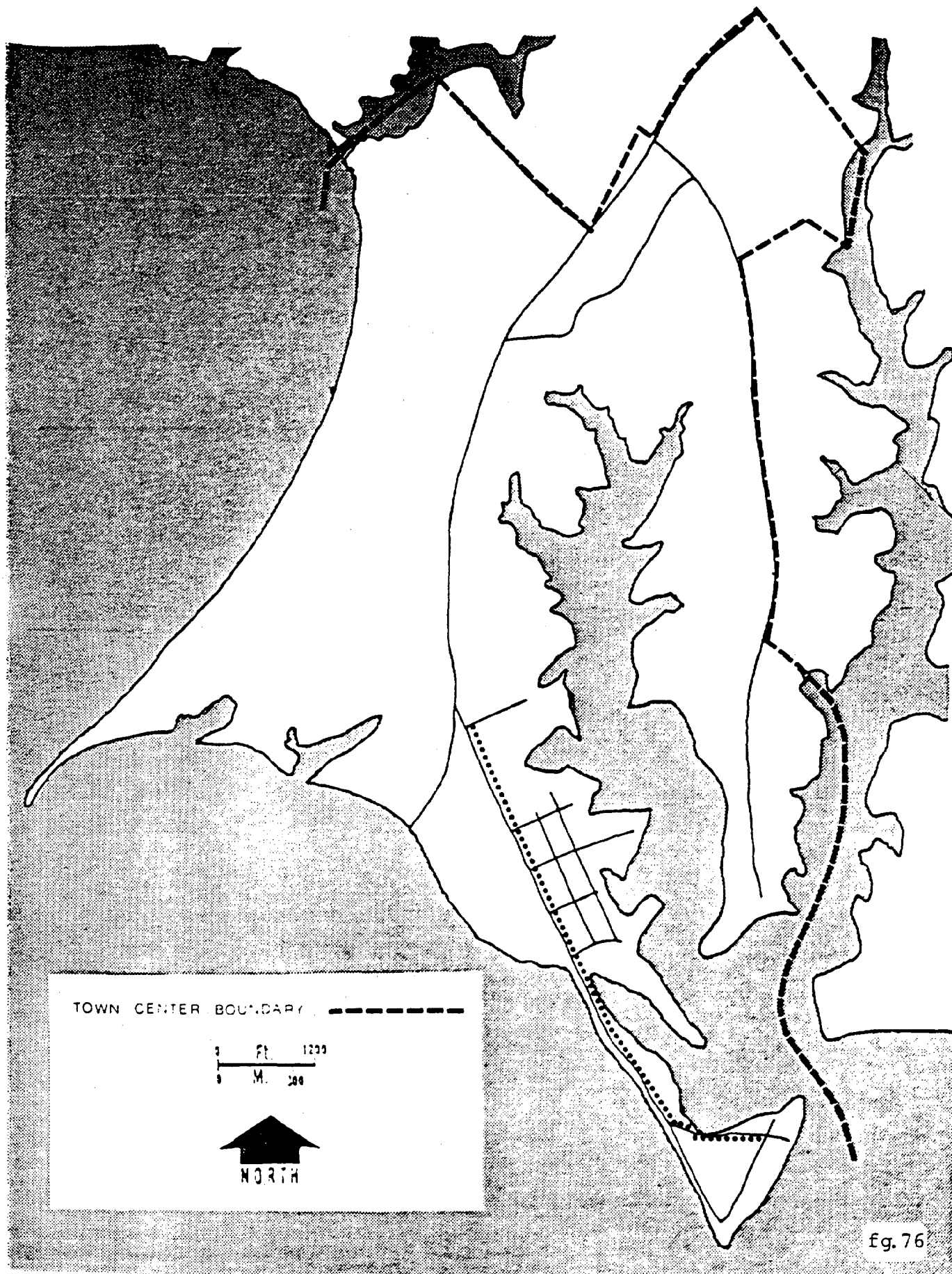


fig. 76

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SIDEWALK



fig. 77

SOLOMONS MASTER PLAN

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Bench Type

8. Bicycle Path.

A major goal of this Master Plan is to alleviate automobile congestion in Solomons and to enhance the pleasant small town environment which currently exists.

Because of its basically level terrain, and relatively small area, the Solomons Town Center is naturally suited to recreational travel by bicycle.

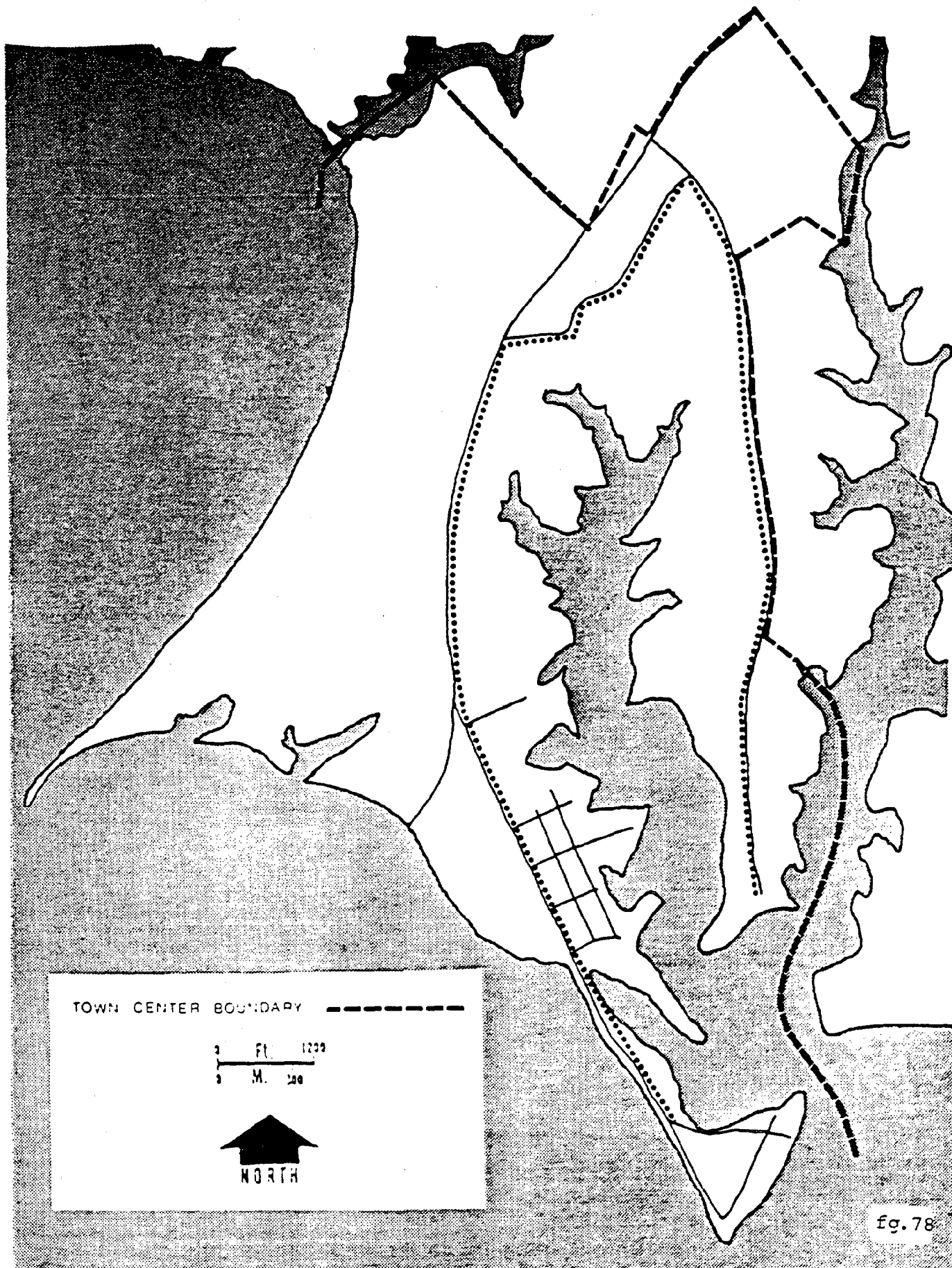
There are several operations which could be united by such a bicycle path:

- o the Naval Recreational area,
- o the Calvert Marine Museum,
- o Zahniser's and Spring Cove Marinas,
- o and the Biological Lab in the Sandy Point area.

Residents of any new development on the Dowell Peninsula could also benefit from such a network.

Recommendation:

Designate a bicycle path for the entire Town center area to provide an alternative means of transportation. Such a path should connect the residential, recreational and parking areas, with the historical commercial area.



SOLOMONS MASTER PLAN

PREPARED FOR CALVERT COUNTY BY THE PHIPPS GROUP, J.M. HUTTO ASSOCIATES AND ALAN SPARBER & ASSOCIATES

BICYCLE PATH

9. Underground Utilities.

The presence of overhead utilities dramatically changes the skyline and clashes with the preservation and enhancement of the historic character of Solomons.

The scheduling of construction of sidewalks provides an opportunity to coordinate the undergrounding of the utilities with this construction, and achieve some cost savings.

Recommendation.

Underground the utilities in the historic Solomons commercial area and along the Solomon's Island Road. While it is preferable that this be carried out as a single project, it may have to be accomplished in two stages because of cost. In that case, the first stage of undergrounding should concentrate on the area of Lore Street South.

10. Museum Parking.

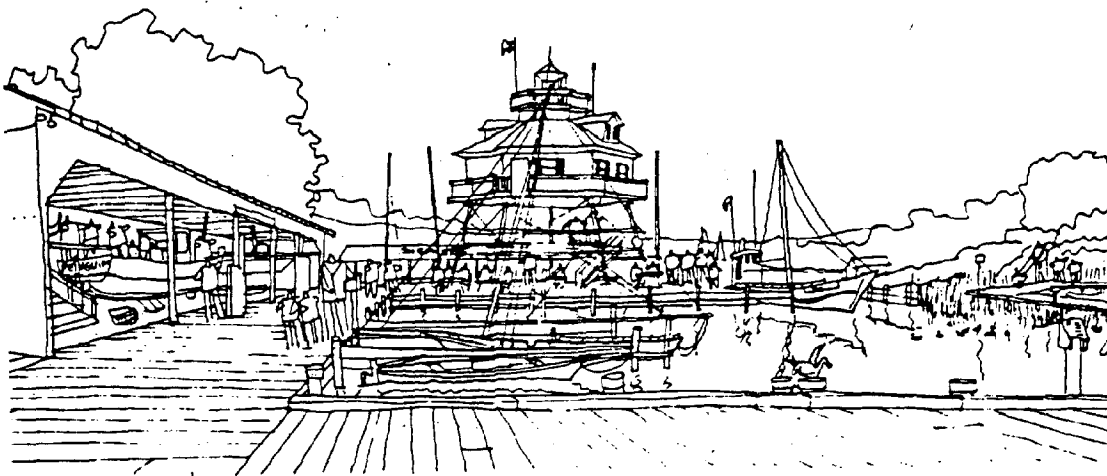
Because of the increasing attendance and future expansion plans of the Calvert Marine Museum, additional parking for the facility will be required.

The museum currently leases land across Solomon's Island Road for this purpose.

Recommendation.

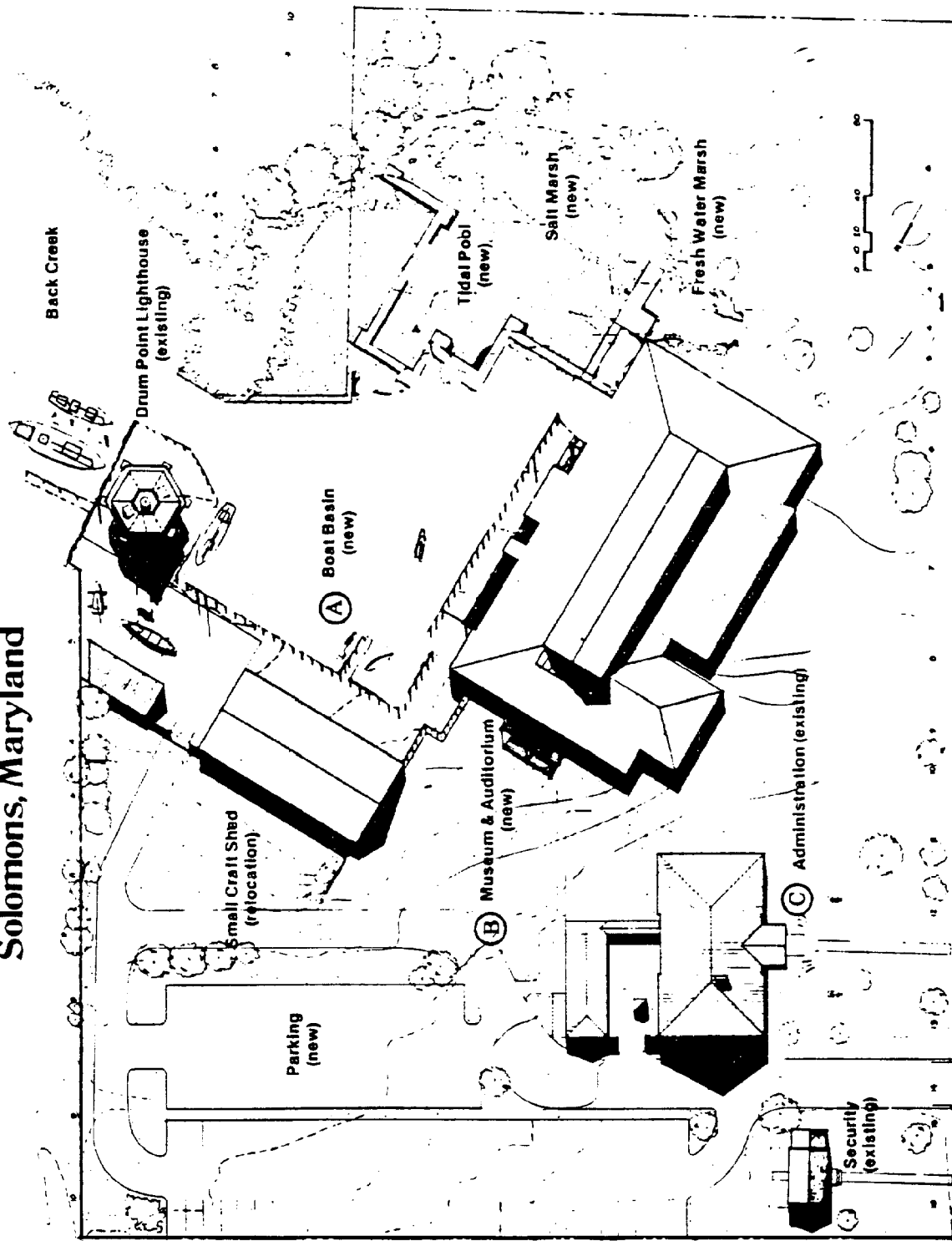
It is recommended that sufficient land be purchased across from the Museum to provide both additional parking space, and the potential for a bus turnaround.

If this proves infeasible, it is recommended that spaces be provided at the State Park facility and shuttle bus service to the Museum and the historic commercial core be investigated.



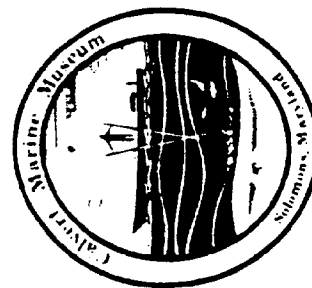
CALVERT MARINE MUSEUM

Solomons, Maryland



The Calvert Marine Museum Board of Governors and the Board of Calvert County Commissioners have approved the following three phase plan:

- (A) Expand waterfront and create fresh and salt water marshes.
- (B) Construct new exhibit building with auditorium facility.
- (C) Renovate present exhibit building for expanded library, conservation laboratory, collection storage, and administration space.



Master Development Plan

fig. 79

11. Shuttle Bus Service.

Mitigation of automobile congestion on the Island is a critical aspect of this plan. With the provision of public parking spaces at the new State Park under the bridge, it would be feasible to encourage some temporary parking at this site, if shuttle bus service to the historic commercial core were available.

Recommendation.

The County should investigate the possibility of providing shuttle bus service to Solomons during the summer season. It should be clearly understood that this service will have to be subsidized to be effective. In order to encourage the greatest possible usage, this service should be either free, or at only nominal cost.

The principal stops on this shuttle circuit should include the Naval Recreational facility; the Calvert Marine Museum; the marinas on Back Creek; the "Riverwalk"; and the University of Maryland's Biological Laboratory.

12. Farren Avenue Turnabout.

A significant portion of Island traffic follows Charles Street to Farren, with the result that many cars penetrate the residential area looking for an exit. In addition, many residents of Solomons enjoy the scenic view from Charles and Farren and need a facility for turning around.

The problem lies in the tight configuration of land and shore at the intersection of Farren and Charles. While there is no ideal solution available immediately, the following recommendation can function in the short term.

Recommendation.

It is recommended that the public street space, located at the northeastern tip of the Island in front of the University of Maryland's Biological Laboratory docks and the commercial marina be immediately designated and stripped as a turnaround. While the turning radius is narrow, it is possible without any additional construction, for the largest size station wagon to make this turn without backing. This area is also the widest available space in public ownership in the vicinity.

Appropriate signage would encourage all traffic to make the circle and not to turn right onto Farren Avenue. No parking would be allowed along Farren Avenue, north of Charles.

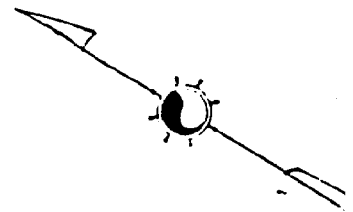
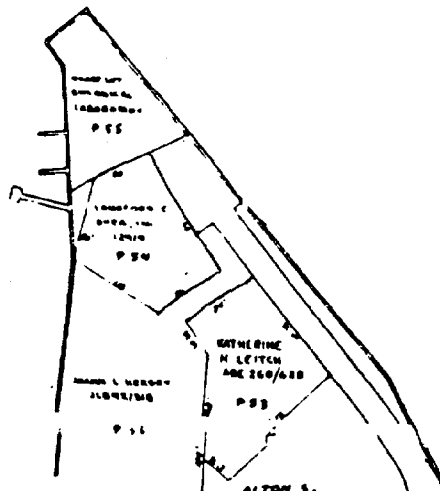


fig. 80

C. TRAFFIC MANAGEMENT

1. Bridge Access Intersection Improvements

Movement by car onto Solomons Island Road from Rt 2/4 can become very congested during peak summer periods. If the recommendations for public improvements provided in this plan are implemented, including the establishment of a park under the Johnson Bridge ramp and public parking across from the Museum, additional congestion can be expected to occur in the same vicinity unless improvements are made to allow better car access to Solomons Island itself. The right turn for traffic coming off the bridge wanting to turn south on Solomons Island Road is satisfactory most of the time but for traffic coming from the north on Rt 2/4, the left turn and stop is confusing and difficult.

Recommendation.

Since plans are underway to realign Rt 2/4 and convert it into a four lane divided highway, we recommend that the intersection be studied carefully by the Maryland Department of Transportation to determine the best way to provide access to Solomons Island Road and the park. For traffic coming from the north on Rt 2/4, one possible solution is to provide a ramp turning to the right off the southbound lanes and crossing under the Johnson Bridge ramp to Solomons Island Road.

2. Street Signage.

It is important to have clear street signs that identify all streets and give appropriate directional information. With the exception of signage along Rt 2/4, current street signs in Solomons are not clear and are sometimes absent, making it difficult and sometimes impossible for visitors to know where they are going.

Recommendation.

A clear metal sign showing street names should be placed at each intersection in Solomons. In some cases this may mean that decisions will need to be made about what name to use for each street. It is strongly recommended that the main road from Lore Street south should be called Solomons Island Road. Other appropriate public signage should be installed including: speed limit signs in the residential and commercial areas; public parking and park directional signs; and activity or vehicle prohibitions (e.g. No Parking, No Fishing).

3. Dowell Road.

Substantial development in the southern portion of the peninsula might generate increased traffic on Dowell Road.

Recommendation.

A traffic analysis of Dowell Road should be done to assess future road improvement needs. A bicycle path is being recommended along Dowell Road to encourage bicycle use and a reduction in car traffic.

4. Traffic in Planning Areas B & C.

The traffic along Solomons Island Road, Charles Street and Farren Ave has been of major concern, especially during the summertime. The area is a focus of recreation and business activity and as a result is the focus of traffic problems.

Recommendations.

There are a number of recommendations that included in this Plan that are designed to reduce traffic concerns:

- o The designation of a parking area across from the Calvert Marine Museum for long term parking. The availability of a shuttle bus and bicycle rentals for movement through the Solomons Island area.
- o A location for parking of boat trailers and recreational vehicles near the Johnson Bridge.
- o The designation of the park under the Johnson Bridge for fishing and the prohibition of fishing along the bulkhead.
- o The building of a sidewalk along the entire length of Solomons Island Road that continues along Charles Street and ends at Farren Ave.
- o The availability of a bicycle path which runs along Solomons Island Road.
- o The suggestions for reducing traffic in the Sandy Point area through curb improvements, and signage.
- o The provision for a car turn-around on Farren Ave to reduce traffic through residential areas.

D. PARKING

1. Public Parking near the Johnson Bridge.

There is a need for public parking in areas that are convenient to major arterials and are away from the busy recreational areas.

Recommendation.

Identify the parking area across from the Calvert Marine Museum as a public parking location. The land could either be leased or purchased from the owner. The parking lot would serve the needs of the Calvert Marine Museum, act as a shuttle bus turn-around, and be a supplementary lot for the bulkhead parking on Solomons Island. A bicycle rental facility could also be located here.

2. Public Parking for Trailers.

Currently the only public space for trailer parking is in the bulkhead parking area. With the implementation of public improvements along the bulkhead, it is recommended that trailer parking in that area be phased out over a period of years.

Recommendation.

There will be a public boat launch in the park that is being proposed under the Johnson Bridge. Parking for trailers will be provided adjacent to the boat launch facility.

3. Bulkhead Parking.

The bulkhead along the Patuxent River has provided parking for customers who visit the restaurants and stores along Solomons Island Road, provided parking for fishermen

who fish off the bulkhead and provided parking for boat trailers.

Recommendation.

The bulkheading from the entrance of Solomons Island to Solomons Pier Restaurant should be expanded. An additional bulkhead south of the Restaurant should be constructed. This expansion and new construction would add between 30% to 40% more parking spaces.

Parking would be restricted to automobiles only, with the following single exception. Five transient boat trailer parking spaces each should be provided for the two existing businesses: H. M. Woodburn's and Dockside. Their use of these spaces would be restricted until the business or property changes hands or changes management or until 5 years after construction of the new bulkhead, at which period this exception should be reviewed and redetermined.

No overnight parking would be permitted.

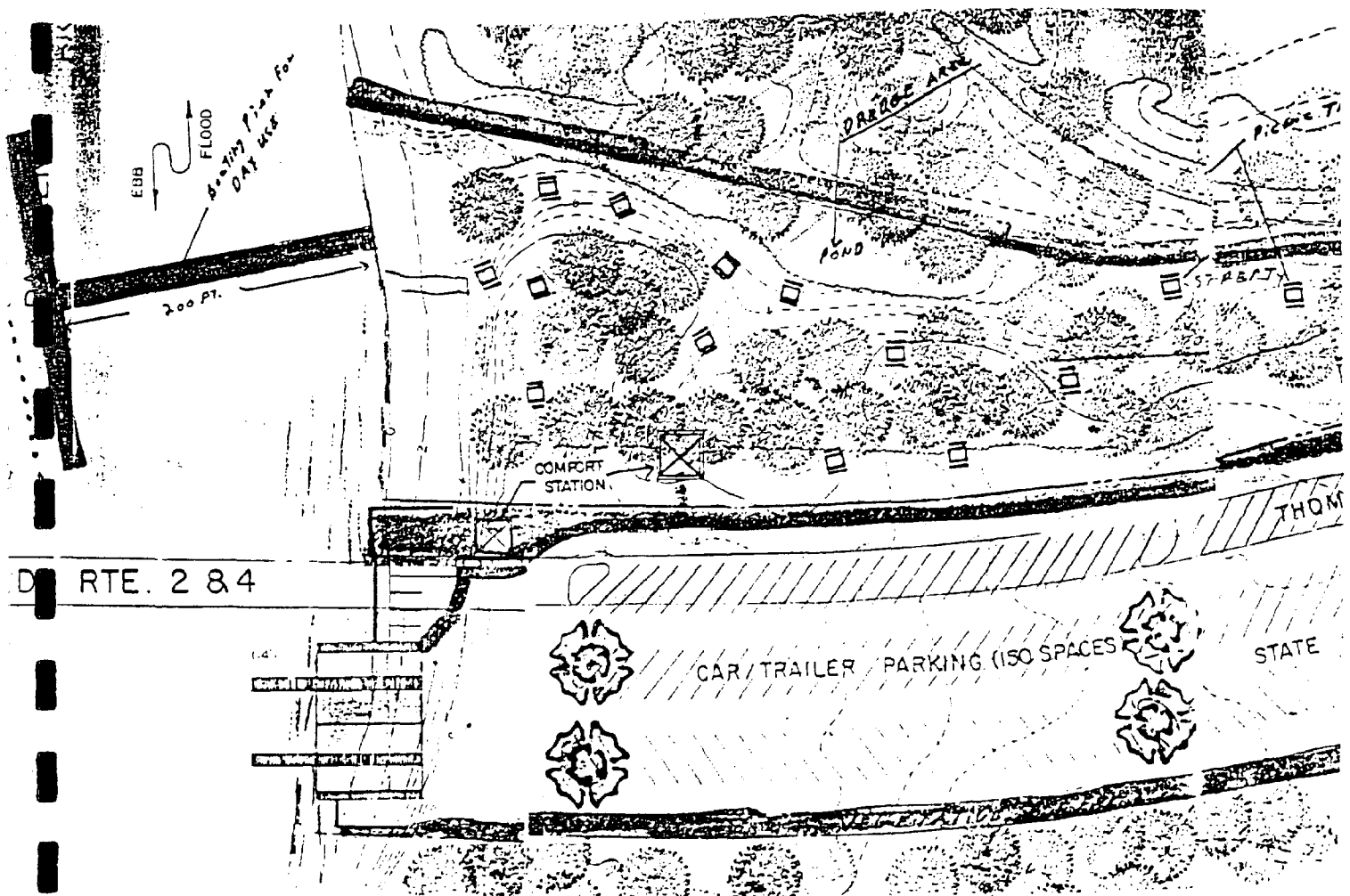
Parking requirements for businesses in Planning Subarea C2 may be satisfied by the non-specific assignment of public parking spaces on the bulkhead.

Public parking spaces on the bulkhead could be used to satisfy a portion of the parking requirements for new commercial development or substantial commercial rehabilitation in Planning Subarea C1 only. A yearly fee would be established for the assignment of such parking spaces on the bulkhead.

E. RECREATION AND PARKS

1. Solomons Center Park.

The State of Maryland Department of Natural Resources is developing land underneath the Johnson Bridge for a boat launch and trailer parking facility. The County has proposed using adjacent land for a park, boating and fishing pier.. The park would contain picnic tables and comfort stations. The park would be an ideal spot for boating and fishing enthusiasts as well as visitors seeking a relaxing spot for a picnic.. It would be away from the commercial and recreational activity of Solomons Island and yet would be convenient from Route 2/4.



2. Molley's Leg.

The history of the small island known as Molley's Leg is filled with conflicting opinions - even to the appropriate spelling of the name. While these differences may date back beyond 1972, the earliest documentation available on the site was from the series of public meetings in 1972 which eventually determined the construction of the current island. Even at that date, the two basic differing opinions seem to fall into the following categories:

- A) keep, restore and enhance Molley's Leg
(there are differing opinions as to how to enhance it) and,
- B) remove Molley's Leg and dispose of the spoil elsewhere.
(there are differing opinions as to where the best place for this would be).

Both opinions claim sound historical, environmental and navigational support, and both claim that their solution would enhance the appropriate tourist draw to Solomons.

Renewed interest in the fate and future of the Island has been expressed in 1984. While the framework of the debate remains basically the same, the two proposed solutions are now expressed in site specific recommendations:

- A) construction of a small replica of the Drum Point Lighthouse in a landscaped setting, and the establishment of a memorial platform to commemorate the waterman;
and
- B) relocation of the entire bulk of Molley's Leg to the northeastern tip of Solomon's Island to provide a park, parking lot and turnaround on the shoreside of the intersection of Charles Street and Farren Avenue.

In August, 1984 the State cleared and leveled the Island at the County's request in order to enhance the navigational safety of the inner harbor for boaters.

Options:

Policymakers at the County level will determine the final resolution of this issue. Among the options which can be considered singly or in concert are the following:

- 1) Do Nothing Option: take no further action regarding Molley's Leg.
- 2) Navigational Management Option: relocate the navigational aids closer to Molley's Leg, thus enlarging the harbor 10 feet on the south side; prohibit anchoring in the Narrows; prohibit anchoring in School House Cove and Provide at cost to pier owners signs that state "No Anchoring within 150 feet."
- 3) Memorial Option: Use the Island surface to commemorate some aspect of Solomon's nautical heritage - whether it be shipbuilding; oystering/crabbing/fishing; or a lighthouse.
- 4) Passive Recreational Option: Plant extensively in order to provide an attractive landscaped view.
- 5) Active Recreational Option: Permit an extension of the Calvert Marine Museum activities to the Island during the Summer Months and make the Island a stop for the excursion boat.
- 6) Commercial Use Option: Transfer all marine gasoline services to the Island and thus reduce the risk of fire near commercial/residential establishments.
- 7) Removal of the Island & Construction of Barrier at Harbor Entrance: Remove all soil and spoil

from Molley's Leg, dredge area and dispose of spoil near the Harbor entrance. Construct appropriate barrier at harbor entrance to protect from storms from the South.

- 8) Removal of Island & Construction of Waterfront Community Park on Front of the Island. Remove all soil and spoil from Molley's Leg, dredge area and dispose of spoil at the front of Solomon's Island near the intersection of Charles Street and Farren Avenues. Create a community park, car turnaround, and memorial.
- 9) Removal of Island & Construction of Bulkhead on Back Creek. Remove all soil and spoil from Molley's Leg, dredge area and dispose of spoil at proposed construction site for the bulkhead and scenic walkway in Back Creek.

Each of the options cited above will have specific costs and benefits associated. It is recommended that as choices are made, a cost/benefit analysis be carried out.

CHAPTER VII
IMPLEMENTATION

CHAPTER VII
IMPLEMENTATION

- A. Introduction
- B. Private Development & Rehabilitation
- C. Staging & Cost of Public Improvements

A. INTRODUCTION.

Implementation of the recommendations in this Solomons Master Plan will depend on strong public and private support. Primary, of course will be approval by the Planning Commission and the County Commissioners of the general policies in the Plan as well as the specific recommendations. Equally important will be their decision to aggressively seek the funding from various sources to implement the public improvements recommended in this Plan. Also significant will be their approval to channel incremental tax dollars generated by new development into the Solomons Town Center to assist in implementing the public improvements through the implementation of a Tax Increment Financing District.

It will also be important for the public to be vigilant and support the enforcement of the necessary rules and regulations to make the general policies viable. Among the rules that need to be consistently monitored are the traffic and parking regulations and the noise regulations.

B. PRIVATE DEVELOPMENT & REHABILITATION.

One of the keys to private development and rehabilitation in the Solomons Town Center is the availability of water and sewer. Adequate water and sewer for the Town, according to current plans, should be available by the end of 1985.

C. STAGING & COST OF PUBLIC IMPROVEMENTS.

Table 2 gives estimates of the cost of the major public improvements recommended in this Plan. It is recommended that the staging of the public improvements occur in the following order:

1. New Sidewalk - Lore Street to Farren Avenue
2. Recreation Center
3. Entry Bridge
4. Bulkhead Improvements - South side of Solomons Pier Restaurant
5. Bulkhead Improvements - North side of Solomons Pier Restaurant
6. Back Creek Bulkhead

TABLE 2
PUBLIC IMPROVEMENT COST ESTIMATES
January 1985

Item	Amount	Unit Cost	Total Cost
1. <u>New Sidewalk - Lore Street to Farren Avenue</u>			
Sidewalk (exposed aggregate)	6,400 lf	\$6/lf	\$38,400
Curb & Gutter	6,400 lf	\$20/lf	128,000
Trees	70	\$600/ea	42,000
Tree Grates	70	\$750/ea	52,500
Street Lights	65	\$2,000/ea	130,000
Benches	15	\$1,000/ea	15,000
Trash Receptacles	10	\$500/ea	5,000
		TOTAL	\$410,900
2. <u>Recreation Center</u>			
Information Building			\$120,000
Bicycle Racks			2,000
Trees	15	\$600/ea	9,000
Tree Grates	15	\$750/ea	9,000
Benches	3	\$1,000/ea	3,000
Street Lights	2	\$2,000/ea	4,000
Trash Receptacles	3	\$500/ea	1,500
		TOTAL	\$148,500

January 1985

\$807,800

TABLE 2
(continued)

PUBLIC IMPROVEMENT COST ESTIMATES

January 1985

Item	Amount	Unit Cost	Total Cost
5. <u>North Side of Solomons Pier Restaurant</u>			
Bulkheading	1,400 lf	\$325/lf	\$455,000
Backfill	17,000 yds ³	\$30/yds ³	510,000
Pier	500 ft ²	\$14/ft ²	7,000
Pavilions	5,000 ft ²	\$35/ft ²	175,000
Riverwalk & Railings	1,400 lf	\$350/lf	490,000
Trees	90	\$600/ea	54,000
Tree Grates	90	\$750/ea	67,500
Street Lights	15	\$2,000/ea	30,000
Benches	35	\$1,000/ea	35,000
Trash Receptacles	15	\$500/ea	7,500
Blacktop	110 pkg spaces	\$400/space	44,000
Landscaping/Shrubs			35,000
TOTAL			\$1,910,000

TABLE 2
(continued)

PUBLIC IMPROVEMENT COST ESTIMATES

January 1985

Item	Amount	Unit Cost	Total Cost
6. <u>Back Creek Bulkhead</u>			
Bulkheading	400 lf	\$325/lf	\$130,000
Backfill	2,000 yds ³	\$30/yds ³	60,000
Boardwalk & Railings	400 lf	\$350/lf	140,000
Trees	30	\$600/ea	18,000
Street Lights	5	\$2,000/ea	10,000
Benches	10	\$1,000/ea	10,000
Trash Receptacles	5	\$500/ea	2,500
TOTAL			\$370,500

